

AGENDA

PLANNING COMMITTEE

WEDNESDAY, 16 SEPTEMBER 2020

1.00 PM

VIA ZOOM CONFERENCING SYSTEM

Committee Officer: Jo Goodrum
Tel: 01354 622285
e-mail: memberservices@fenland.gov.uk

Due to the Covid-19 outbreak and the restrictions by the Government on gatherings of people, this meeting will be conducted remotely using the Zoom video conferencing system. There will be no access to this meeting at the Council offices, but there will be public participation in line with the procedure for speaking at Planning Committee.

The meeting will be available to view on YouTube: [URL](#)

- 1 To receive apologies for absence.
- 2 To report additional items for consideration which the Chairman deems urgent by virtue of the special circumstances to be now specified
- 3 To receive Members declarations of any interests under the Local Code of Conduct or any interest under the Local Code of Conduct or any interest under the Code of Conduct on Planning Matters in respect of any item to be discussed at the meeting.
- 4 F/YR20/0417/F
Storage Buildings and Outbuildings East of 5-6 Norfolk Street, Wisbech, Conversion of existing store building to 3 x 1-bed flats including insertion of 5no windows and conversion of part of No.6 to 1 x single-storey flat involving extension and raising of roof height (Pages 3 - 16)

To determine the application.

- 5 F/YR20/0427/F
Land North Of Manea Railway Station Access Via, Fodder Fen Road,
Manea. Formation of a car park (to serve Manea Train Station) and the formation of
an access and 2m (max) wide footpath link (Pages 17 - 32)

To determine the application.

- 6 F/YR20/0531/VOC
The College Of West Anglia, Elm High Road, Wisbech, Variation of condition 20 to
enable amendment to approved plans of planning permission F/YR16/0792/F
(Erection of 137 dwellings, alterations to Ramnoth Road and Elm High Road junction
to form a new vehicular & pedestrian access and associated works & infrastructure)
(Pages 33 - 58)

To determine the application.

- 7 F/YR20/0576O
Land North West Of 8, Jobs Lane, March, Erect 1 x dwelling (outline application with
matters committed in respect of access) (Pages 59 - 68)

To determine the application.

- 8 Planning Appeals. (Pages 69 - 72)

To consider the appeals report

- 9 Items which the Chairman has under item 2 deemed urgent

Members: Councillor D Connor (Chairman), Councillor A Hay (Vice-Chairman), Councillor I Benney,
Councillor S Clark, Councillor M Cornwell, Councillor A Lynn, Councillor C Marks, Councillor
Mrs K Mayor, Councillor N Meekins, Councillor P Murphy, Councillor R Skoulding and
Councillor W Sutton,

F/YR20/0417/F

Applicant: Mr Colin Hawes

Agent : Mr Colin Hawes

Storage Buildings and Outbuildings East of 5-6 Norfolk Street, Wisbech

Conversion of existing store building to 3 x 1-bed flats including insertion of 5no windows and conversion of part of No.6 to 1 x single-storey flat involving extension and raising of roof height

Reason for Committee: Town Council comments contrary to officer recommendation

1. EXECUTIVE SUMMARY

- 1.1. The proposal is for the conversion of existing outbuildings to the rear of commercial premises into 4 residential flats.
- 1.2. The property is located in the town centre, within walking distance of services and public transport links.
- 1.3. Objections from the Town Council and the Wisbech Society relate to matters of overdevelopment of the site, lack of off-street parking and impact on the appearance of the building in the Conservation Area.
- 1.4. The town centre location is considered sufficient to mitigate the lack of parking provision, whilst the visual impact of the scheme on the conservation area is minimal. Conditions are considered sufficient to mitigate the impacts of the proposal.
- 1.5. The proposed units are of a modest size with no private outdoor amenity space, however this is common for flats in such locations, and amendments have been made to the scheme to ensure the relationships between the proposed units of accommodation are acceptable.
- 1.6. The recommendation is therefore to grant planning permission for the proposal.

2. SITE DESCRIPTION

- 2.1. The application site is the land to the rear of 5-6 Norfolk Street in Wisbech, and comprises outbuildings and a storage building associated with the commercial premises fronting Norfolk Street.
- 2.2. The arrangement of the existing buildings is such that the site is not visible from Norfolk Street as it is located behind the traditional commercial buildings. From

the east the only visible part of the proposal is a narrow section of the brick storage building with a timber door at the ground floor level.

- 2.3. The courtyard to the rear of Norfolk Street contains three buildings. They are all of brick construction with mono-pitched roof structures, however each of the three buildings has a different depth. As such, they appear as a series of narrowing buildings typical of traditional burgage plots in such areas.

3. PROPOSAL

- 3.1. The proposal is for the conversion of the buildings to create a total of four new residential units. 3 of these would be created in the easternmost building, whilst the fourth would be created by the conversion and extension of the existing outbuildings at the rear of the Norfolk Street property.
- 3.2. Full plans and associated documents for this application can be found at:

4. SITE PLANNING HISTORY

F/YR15/0487/PNC07	Change of Use of Storage Building to 2 x 2-storey 1-bed dwellings	Refused
F/YR15/0264/F	Conversion of store to form 2 x 2-storey 1-bed dwellings	Withdrawn

5. CONSULTATIONS

5.1. **Wisbech Town Council**

Objection – overdevelopment of the site and no provision for on-site parking

5.2. **Conservation Officer**

Proposal seeks to utilise a former redundant storage building through clever use of the stairwell within Orange Grove to provide access. Nothing architecturally or historically noteworthy about the brick elevation to the east and no issue with a new brick skin being introduced. Proposals cannot be seen from Norfolk Street. The revisions remove the concerns arising originally from the introduction of a mansard roof to the central conversion.
Recommend approval.

5.3. **FDC Environmental Health:**

No objection in principle. Detail should be provided demonstrating how suitable noise and ventilation measures can be achieved.

A further response was received following correspondence with the agent stating that they were satisfied with the proposal.

5.4. **Cambridgeshire County Council Highways Authority:**

No highway objections. FDC will need to consider impact on parking in the area.

5.5. **Cambridgeshire County Council Historic Environment Team (Archaeology)**

No likely objection in principle. Request additional information regarding the current and historic functions of the building to account for the decorative treatment of the storage building. A photographic record of the existing condition would be helpful.

5.6. **The Wisbech Society**

Objection. Proposal involves masking the existing distinctive brick bands in the building. 3rd storey accommodation will result in the loss of the existing steep pitched roof, which should be retained. Visual changes should be kept to a minimum to protect historic features. Don't support the use of uPVC in Conservation Areas. Any extension to the building may require archaeological investigation. Overdevelopment of the site. Replacement of pantiles with slates will change the character of the building.

5.7. **Local Residents/Interested Parties:**

One response has been received to the proposal, from the owner of the neighbouring property noting their support for the scheme and citing the following justification.

Will provide needed small unit accommodation. Elevation to Orange Grove will be significantly improved.

6. **STATUTORY DUTY**

6.1. Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires a planning application to be determined in accordance with the Development Plan unless material planning considerations indicate otherwise. The Development Plan for the purposes of this application comprises the adopted Fenland Local Plan (2014).

6.2. Section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires Local Planning Authorities when considering development to pay special attention to the desirability of preserving or enhancing the character or appearance of a conservation area.

7. **POLICY FRAMEWORK**

National Planning Policy Framework (NPPF)

Para 2: NPPF is a material consideration

Para 8: 3 strands of sustainability

Para 11: Presumption in favour of sustainable development

Para 117: Promote effective use of land

Para 118: Opportunities and benefits of the reuse of land

Para 121: Take a positive approach to alternative land uses

Para 184: Heritage assets should be conserved in a manner appropriate to their significance.

Para 189: Applicants should describe the significance of any heritage assets affected.

Para 196: Less than substantial harm should be weighed against public benefits.

National Planning Practice Guidance (NPPG)

Determining a Planning Application

National Design Guide 2019

Context

Identity

Built Form

Uses

Fenland Local Plan 2014

LP1 – A Presumption in Favour of Sustainable Development

LP2 – Facilitating Health and Wellbeing of Fenland Residents

LP3 – Spatial Strategy, the Settlement Hierarchy and the Countryside

LP4 – Housing

LP15 – Facilitating the Creation of a More Sustainable Transport Network in Fenland

LP16 – Delivering and Protecting High Quality Environments across the District

LP18 – The Historic Environment

8. KEY ISSUES

- **Principle of Development**
- **Residential Amenity**
- **Impact on Historic Character/Features**
- **Parking**

9. BACKGROUND

- 9.1. Prior approval was sought under Class P of the Town and Country Planning (General Permitted Development) Order 2015 for the conversion of the storage building to the rear of the site into two dwellings in 2015, however the application was refused on the basis that insufficient evidence had been provided to demonstrate the building was within a B8 use class and therefore it failed to qualify for the prior approval procedure. A previous full application was withdrawn prior to determination.

10. ASSESSMENT

Principle of Development

- 10.1. Policy LP3 of the Fenland Local Plan (2014) sets out the settlement hierarchy within the District, setting out the scale of development appropriate to each level of the hierarchy.
- 10.2. Wisbech is a Primary Market Town, one of two such settlements within the highest level of the hierarchy where the majority of the development within the District is expected to take place over the plan period.
- 10.3. The proposal involves the conversion of the existing buildings on the site, including a modest extension to one of those buildings, to provide residential accommodation. This is within the scope of the type of development envisaged within the Primary Market Town level of the settlement hierarchy and therefore the principle of the scheme is considered to be acceptable.

Residential Amenity

- 10.4. Policy LP2 of the Fenland Local Plan (2014) requires development proposals to promote high levels of residential amenity, and policy LP16 requires development proposals to demonstrate that they do not adversely impact on the amenity of neighbouring users whilst providing sufficient amenity space for the proposal, with the guideline for non-flat development being one third of the plot area.

- 10.5. As the proposal is for the development of flats there is no guideline minimum for the amount of amenity space associated with the proposed units. None of the units proposed are indicated as having any private outdoor amenity space. On the basis that the proposal is for the creation of flats, there is no policy presumption in the development plan that the lack of private amenity space should be a reason for refusal of the scheme. The proposed flats in the 3-storey building have no communal space provision, whilst the remaining proposed unit would have a small yard area shared between itself and numbers 5 and 6 Norfolk Street. The Town Centre location of the development mitigates the lack of communal space provision due to the proximity to its facilities and services.
- 10.6. The arrangement of the units has been amended at the request of the Local Planning Authority to remove a fifth flat from the scheme created through the addition of a second floor to the outbuildings. This fifth unit had resulted in a privacy conflict between bedroom windows within the development that has now been resolved by the revised layout.
- 10.7. The floorspace within the proposed flats is limited, however the proposal is for the provision of small flats typical of a Town Centre location.
- 10.8. On that basis, there is considered to be no justification for refusal of the scheme on the grounds of residential amenity, which is proportionate to the type of properties proposed.
- 10.9. The proposed plans do not show specific arrangements for the provision of bins associated with the proposed residential units, however the yard area available at the rear of 6 Norfolk Street and the existing collections arrangements for the Orange Grove flats adjacent to the site, which are also within the applicant's ownership are sufficient to demonstrate that appropriate arrangements can be made, and therefore a condition can be imposed on any permission requiring details to be provided and agreed prior to occupation of the flats.

Impact on Historic Character/Features

- 10.10. Policy LP18 addresses matters concerning the historic environment within Fenland, noting that development proposals will be required to describe and assess the significance of any heritage asset, identify the impact of proposed works on its character and provide justification for those works, especially if they would harm the setting of the asset.
- 10.11. The application site is located in the Conservation Area, and the Cambridgeshire County Council Historic Environment Team have identified an unusual decorative pattern within the brickwork at the ground floor level of the eastern elevation of the storage building to be converted. It should be noted that they have not raised an objection to the proposal based on the impact on this wall.
- 10.12. The majority of works proposed to be undertaken in relation to the scheme will not be visible from any point outside the site, with only the proposed alterations to the eastern elevation of the storage building being publicly visible. These alterations consist of the forming of a new brick skin outer wall to allow the existing wall to form the inner skin of a cavity wall. The more modern upper part of this brick wall would then be removed to allow the insertion of a new angled roof to match the appearance of the adjacent building.

- 10.13. The Conservation Officer has raised no objection to the proposal on the basis of its impact on the Conservation Area, and whilst the comments of the Cambridgeshire County Council Historic Environment Team are noted, it is considered that a condition requiring a photographic record to be made of the premises prior to the construction of the new brick skin will adequately mitigate the impact of the scheme.
- 10.14. Overall there is considered to be no harm to the Conservation Area arising from the proposal.

Parking

- 10.15. Appendix A of the Fenland Local Plan sets out the parking standards associated with development proposals, noting that for flats the parking requirement is 1.25 spaces per single-bedroom unit, and 1.5 spaces in other cases.
- 10.16. It also notes however that in some circumstances a reduced or nil parking provision may be appropriate, particularly in circumstances such as sites in the central area of a market town where good public transport links are available.
- 10.17. The proposal makes no provision for on-site parking and there is no scope to accommodate parking on the site. There is on-street parking along Norfolk Street however this is not allocated to the property or the proposed developments and could not therefore be relied on to provide parking for the development. It is however located within the Town Centre Boundary within walking distance of the Horsefair Bus Station and Car Park, and only 1-2 minutes' walk from St Peter's Car Park to the north west.

11. CONCLUSIONS

- 11.1. The proposal is for the conversion of existing outbuildings within the site in a town centre location. The scheme will have a limited impact on the character and appearance of the conservation area that will result in no harm, subject to the use of appropriate materials.
- 11.2. There is no parking provision associated with the scheme, however the location of the site in close proximity to the town centre and public transport links means that this is acceptable.
- 11.3. The proposed residential units have limited amenity and are of a limited size, however this is appropriate to the nature of the accommodation they are providing.

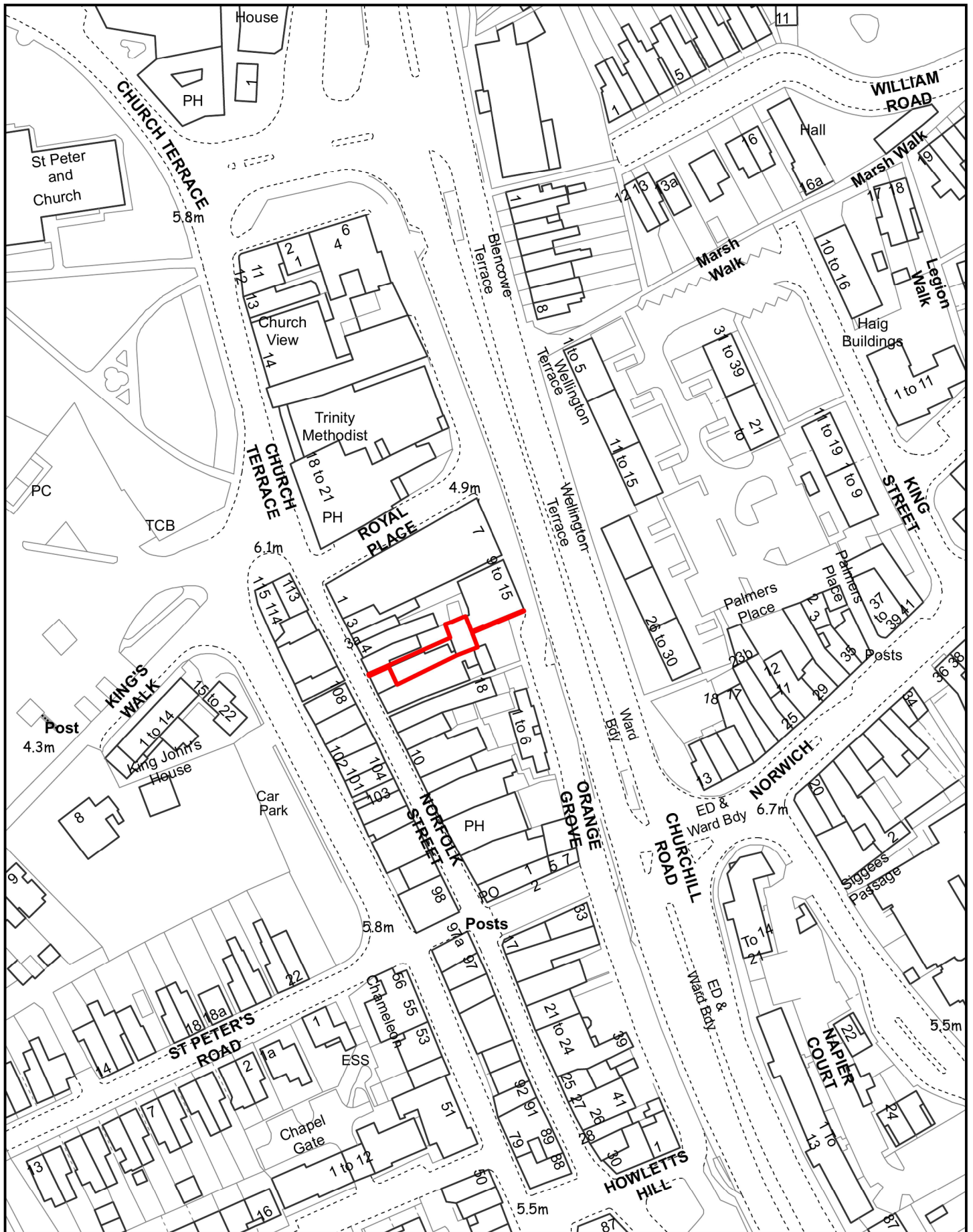
12. RECOMMENDATION

Grant planning permission, subject to the following conditions

- | | |
|----|---|
| 1. | The development permitted shall be begun before the expiration of 3 years from the date of this permission.

Reason: To ensure compliance with Section 51 of the Planning and Compulsory Purchase Act 2004. |
| 2. | No development other than groundworks and foundations shall take place until full details of the materials to be used in the development hereby approved for the walls and roof are submitted to and approved in |

	<p>writing by the Local Planning Authority. The details submitted for approval shall include the name of the manufacturer, the product type, colour and reference number. The development shall then be carried out in accordance with the approved details and retained in perpetuity thereafter.</p> <p>Reason: To safeguard the visual amenities of the area in accordance with Policy LP16 of the Fenland Local Plan, adopted May 2014.</p>
3.	<p>No works in connection with the conversion of the storage building to three single-bedroom flats hereby approved shall commence until a photographic survey of the existing building and structures on the application site has been submitted to and approved in writing by the planning authority. All external and internal elevations of the buildings and structures together with the setting of the buildings and structures shall be photographed. The photographic viewpoints must be clearly annotated on a plan to accompany the survey. The photographs and plan must be in a digital format and must be clearly marked with the planning reference number.</p> <p>Reason: To ensure that a record of the building is made prior to any works that would obscure historic features in accordance with policy LP18 of the Fenland Local Plan (2014).</p>
4.	<p>Prior to the first occupation of the development hereby approved a refuse collection strategy shall be submitted to and approved in writing by the Local Planning Authority. The approved refuse collection strategy shall be implemented in accordance with the agreed details in full and thereafter be retained in perpetuity unless otherwise agreed in writing.</p> <p>Reason: To ensure a satisfactory form of refuse collection and compliance with Policy LP16 of the Fenland Local Plan, adopted May 2014.</p>
5.	<p>The development hereby permitted shall be carried out in accordance with the following approved plans and documents</p>



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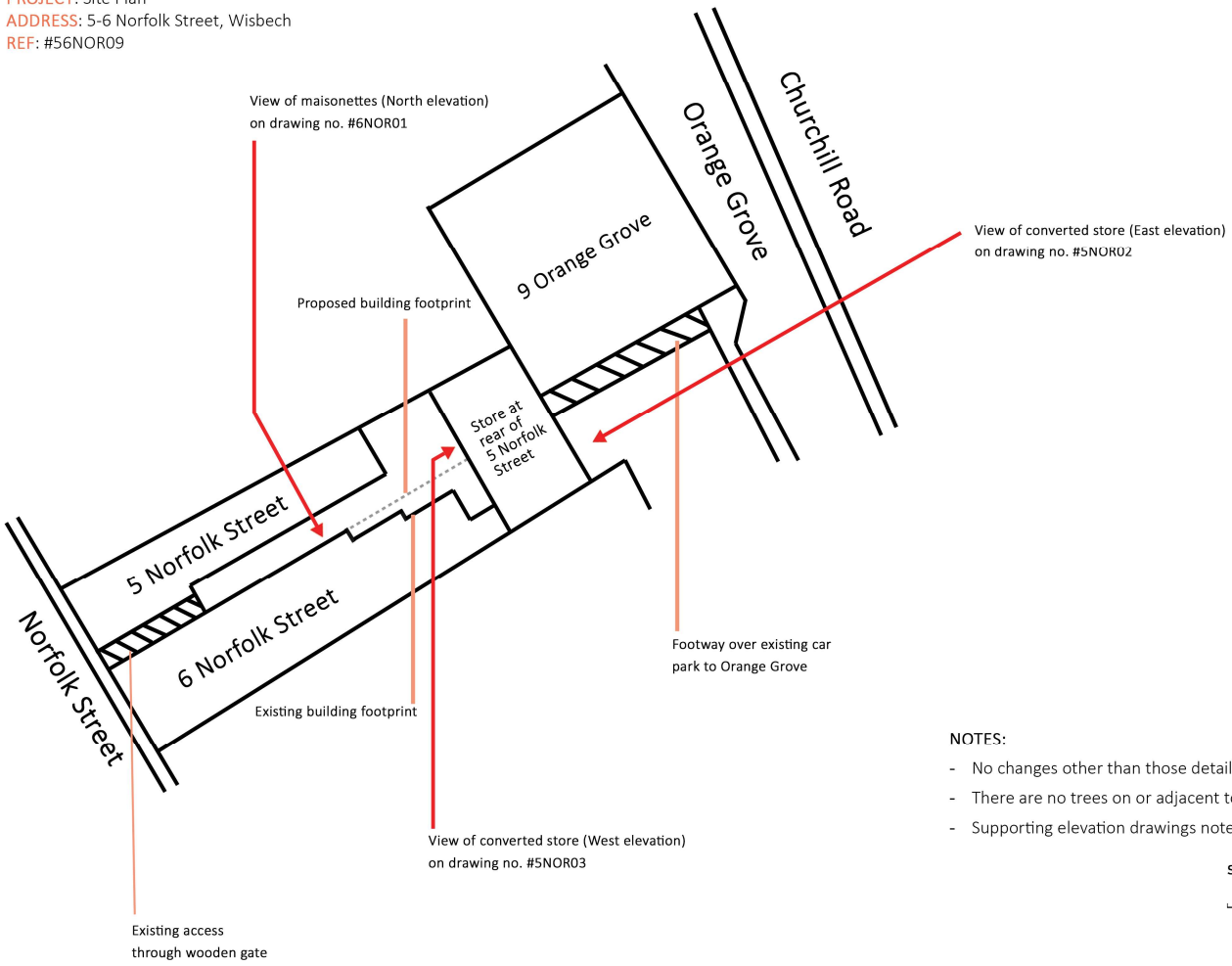
F/YR20/0417/F

Scale = 1:1,250



PROJECT: Site Plan
ADDRESS: 5-6 Norfolk Street, Wisbech
REF: #56NOR09

AUTHOR: Colin Hawes
DATE: 11/05/2020
DOC SIZE: A3
VERSION: 4



NOTES:

- No changes other than those detailed are proposed
- There are no trees on or adjacent to the site
- Supporting elevation drawings noted with red lines below

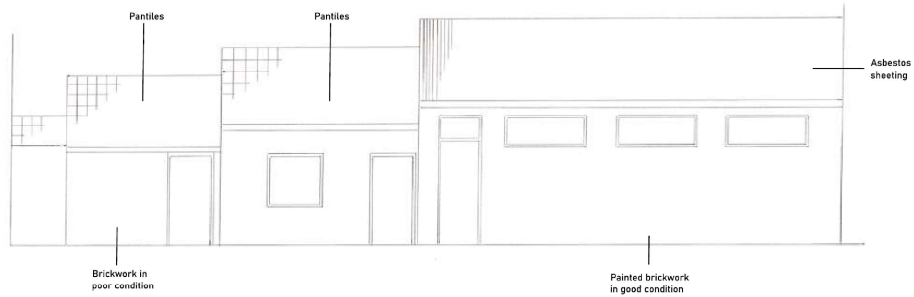
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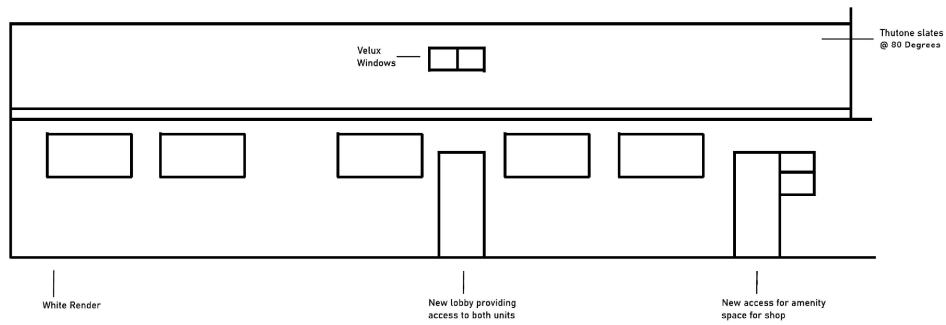
PROJECT - REAR OF 6 NORFOLK STREET, WISBECH

North Elevation

EXISTING



PROPOSED



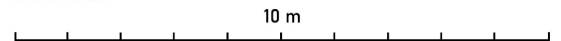
DETAILS

Colin Hawes | Hempnall House, Lundy Green, NR15 2NU
T: 01508 499769 | M: 07802 706826

PROJECT

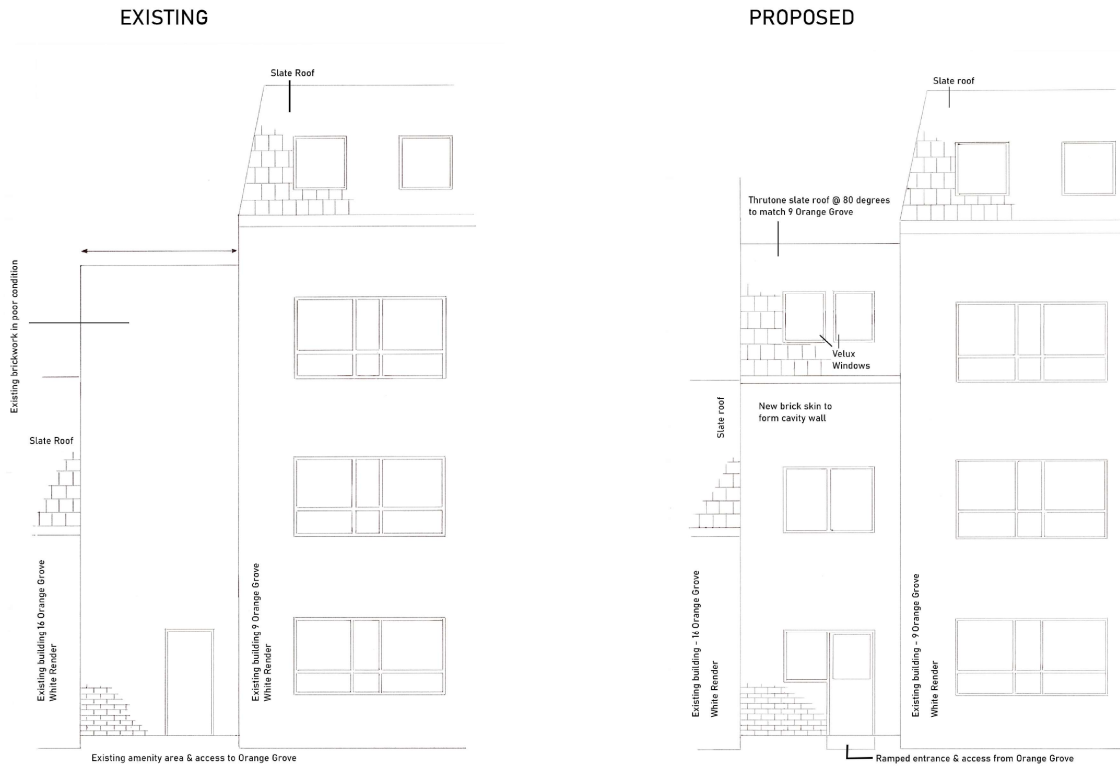
REF: #6NOR01 | Version: 4 | Date: 17/07/2020
Author: CJH | File Size: A4

Scale 1:100



PROJECT - REAR OF 5 NORFOLK STREET, WISBECH

East Elevation



DETAILS

Colin Hawes | Hempnall House, Lundy Green, NR15 2NU
T: 01508 499769 | M: 07802 706826

PROJECT

REF: #5NOR02 | Version: 3 | Date: 04/05/2030
Author: CJH | File Size: A4

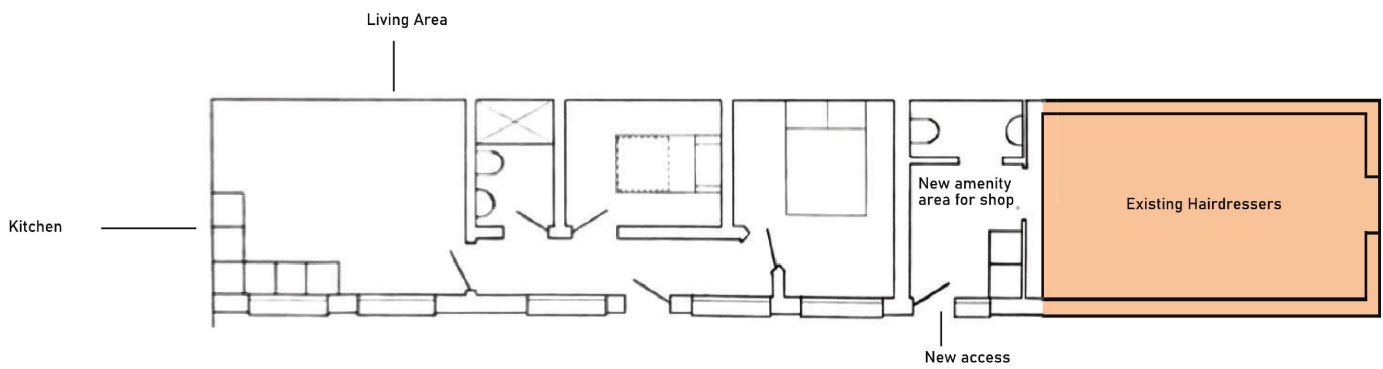
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10 m



PROJECT - REAR OF 6 NORFOLK STREET, WISBECH

GROUND FLOOR PROPOSED



EXISTING



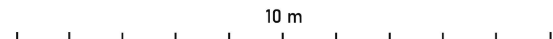
DETAILS

Colin Hawes | Hempnall House, Lundy Green, NR15 2NU
T: 01508 499769 | M: 07802 706826

PROJECT

REF: #6NOR06 | Version: 3 | Date: 17/07/2020
Author: CJH | File Size: A4

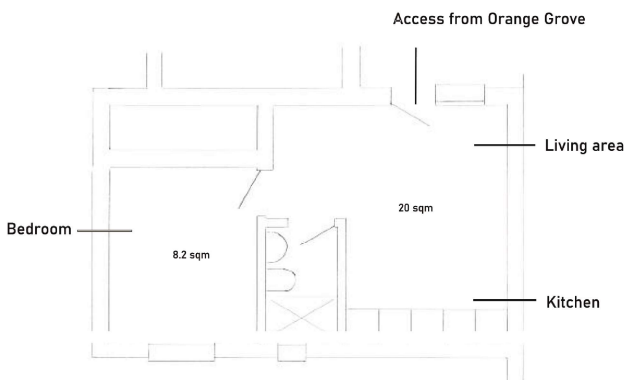
Scale 1:100



PROJECT: Proposed Ground, 1st & 2nd Floor Plans
ADDRESS: Rear of 5 Norfolk Street, Wisbech
Scale: 1/100
REF: #5NOR05

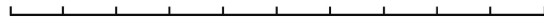
PROPOSED

GROUND FLOOR



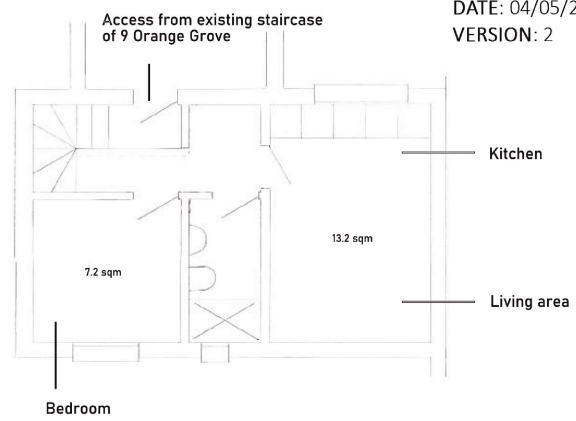
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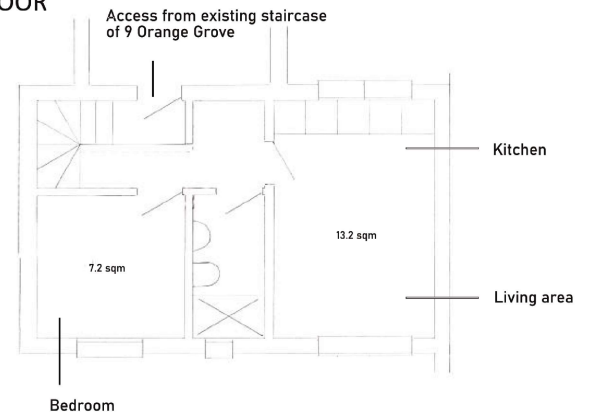


FIRST FLOOR

AUTHOR: Colin Hawes
DATE: 04/05/2020
VERSION: 2

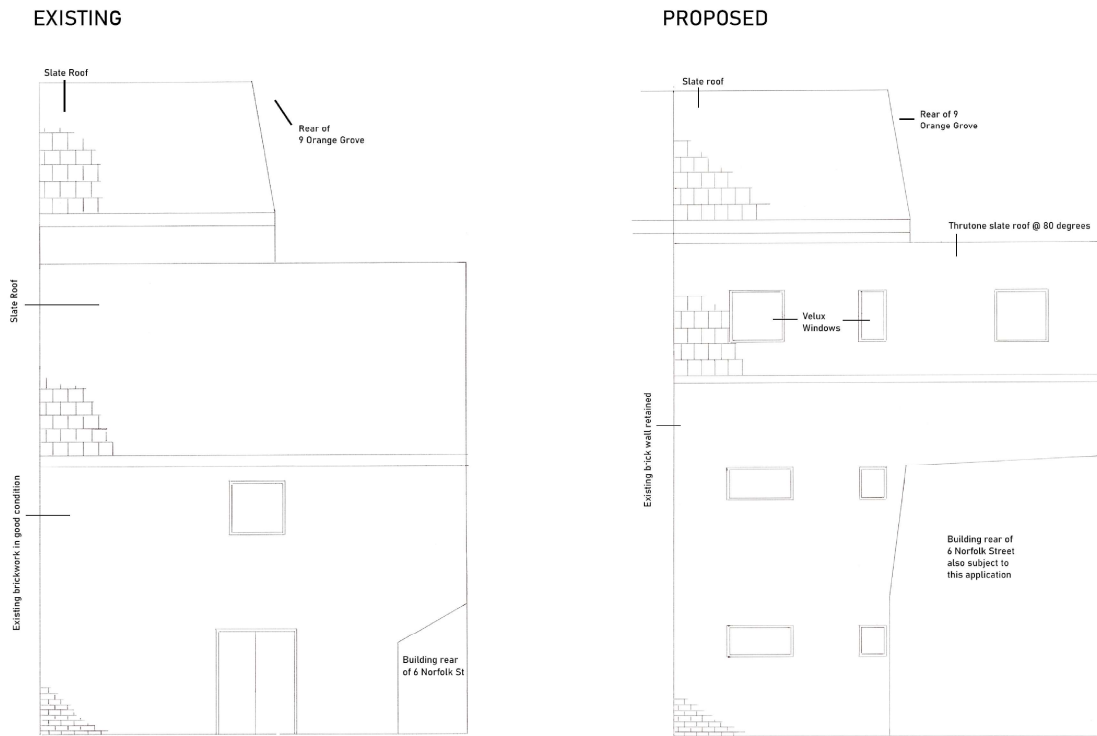


SECOND FLOOR



PROJECT - REAR OF 5 NORFOLK STREET, WISBECH

West Elevation



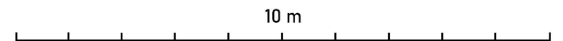
DETAILS

Colin Hawes | Hempnall House, Lundy Green, NR15 2NU
T: 01508 499769 | M: 07802 706826

PROJECT

REF: #5NOR03 | Version: 3 | Date: 04/05/2030
Author: CJH | File Size: A4

Scale 1:100



F/YR20/0427/F

**Applicant: Ms Wendy Otter
Fenland District Council**

Agent :

Land North Of Manea Railway Station Access Via, Fodder Fen Road, Manea,

Formation of a car park (to serve Manea Train Station) and the formation of an access and 2m (max) wide footpath link

Officer recommendation: Grant

Reason for Committee: Fenland District Council are the applicants

1 EXECUTIVE SUMMARY

- 1.1 The application seeks full planning permission for a 112 space car park in association with Manea railway station, including the formation of a new access off Fodder Fen Road and 2m (max) wide footpath link to the station. Surrounding the car park is a 6m buffer from the boundary hedging and a 30m buffer from the overgrown area to the north, required for ecology purposes.
- 1.2 Policies LP1, LP2 and LP15 of the Fenland Local Plan seek to encourage delivery of a more sustainable transport network. The Fenland Infrastructure Delivery Plan strives to improve rail service usage and proposes a car park near Manea station to support this aim.
- 1.3 The application site is considered to relate more to the countryside than the built settlement and as such it is important that this character is retained as much as possible to limit the impact. The formalisation of the access and creation of a 2m (max) wide footway would have an urbanising impact, however this would be outweighed by the public benefit of the scheme and need to ensure that well designed, safe and convenient access for all is achieved. The retention and enhancement of the boundary vegetation will mitigate the impact of car park from the streetscene.
- 1.4 There are a number of dwellings surrounding the site and there is potential that external lighting could have an adverse impact on residential amenity. It is therefore felt necessary to impose a condition in relation to lighting, to ensure adequate mitigation is provided.
- 1.5 Community safety concerns have been raised; security measures such as CCTV, lighting (as mentioned above) along with the management and maintenance of the site have not been confirmed, hence it is necessary to impose a condition in this regard to ensure suitable measures are put in place.
- 1.6 The Local Highway Authority have no objections to the proposal subject to conditions ensuring the provision of the access and footpath. Full details of the footpath design have not been provided and there is potential for this to have an impact on surrounding trees, hence a condition will be imposed to ensure adequate mitigation is secured.

- 1.7 The proposed works will result in the loss of a small area of foraging/commuting habitat, however impacts are not anticipated to be significant. Conditions will be imposed in relation to the retention of the existing boundary vegetation and buffer zone in addition to the recommended ecology enhancement/compensatory measures.
- 1.8 The principle of this development is supported. Subject to the retention and enhancement of the boundary vegetation the proposal is not considered to have a significant detrimental impact on the character of the area. There are no further requirements in relation to archaeology and it is considered that suitable drainage, landscaping, security, lighting and ecology mitigation can be achieved. It is therefore recommended to grant the application

2 SITE DESCRIPTION

- 2.1 The application site is a roughly triangular shaped piece of land located to the north-east of the Manea railway station, on the western side of Fodder Fen Road. The site is bounded by trees and vegetation and is partially overgrown, there are electricity lines which run through the site and an informal gated access off Fodder Fen Road. The site falls within Flood Zone 3, the highest risk of flooding.

3 PROPOSAL

- 3.1 The application seeks full planning permission for a 112 space car park in association with Manea railway station, including the formation of a new access off Fodder Fen Road and 2m wide footpath link to the station. Surrounding the car park is a 6m buffer from the boundary hedging and a 30m buffer from the overgrown area to the north, required for ecology purposes.
- 3.2 Full plans and associated documents for this application can be found at:

<https://www.fenland.gov.uk/publicaccess/applicationDetails.do?activeTab=documents&keyVal=Q8XIE8HE03000>

4 SITE PLANNING HISTORY

F/YR14/0113/F	Erection of 3no dwellings comprising of 1 x 2-storey 4-bed with detached double garage/workshop/store, 1 x 2-storey 4-bed with attached garage with store above and 1 x 4-bed with attached double garage	Refused 01/07/2014 Dismissed on Appeal 09/01/2015
F/YR12/0936/F	Erection of 10 x 2-storey dwellings comprising of: 7 x 3-bed dwellings with associated parking, 1 x 3 -bed dwelling with integral garage, 1 x 4-bed dwelling with detached double garage and store, 1 x 4-bed dwelling with an attached double garage, a biomass cabin and the formation of 2 x accesses, a pond and 18 carparking spaces	Refused 09/07/2013

F/YR02/1455/F	Erection of 2 workplace homes	Refused 25/6/2003
F/YR02/0782/O	Erection of 2 dwellings	Refused 21/8/2002
F/YR01/0121/O	Erection of a 4-bed detached house	Refused 16/3/2001
F/YR01/0786/O	Erection of a 4-bed house	Refused 14/9/2001

5 CONSULTATIONS

5.1 Parish Council (16/6/2020)

Members approved the application, however with comments:

- *Pedestrian routes should be fully accessible and DDA compliant.*
- *Taxi pick up and drop off*
- *Cycle park and separation*
- *Turning provision for replacement bus*
- *Bus stop on Fodder Fen Road for future shuttle bus drop off/pick up*
- *Access control*
- *All services supplies should be fully sustainable*
- *Ablution facilities*
- *Rainwater run off (SUDS)*

Separate entry/egress to be considered for future increase in traffic and to reduce queuing on the highway when the level crossing is closed.

5.2 Parish Council (1/9/2020)

Members received the revised proposals and Ecology Report. Members are still concerned that their previous comments are still not included in the revised proposals.

5.3 Designing Out Crime Team (1/6/2020)

Thank you for the opportunity to comment on this application. I have viewed the documents in relation to crime, disorder and the fear of crime and have completed a crime and incident search for Manea covering the last 12 months. I would consider this to be an area of low vulnerability to the risk of crime.

I do have two questions: -

Will the car park be closed over-night after the last trains?

What lighting is being proposed?

I have no further comment or objections at this time.

5.4 Designing Out Crime Team (20/8/2020)

Thank you for the opportunity to comment on this application and revised proposals. I have viewed the documents in relation to crime, disorder and the fear of crime and note the responses in the supporting statement dated 16/06/2020 relating to lighting and operation of the car park.

I have no further comment at this time.

5.5 Environmental Health (FDC)

It would also be reasonable to expect the applicant to provide a full light impact assessment to demonstrate that proposed lighting is in accordance with the Institution of Lighting Professionals Guidance Notes for the Reduction of Obtrusive Light GN01:2011. The accompanying report shall include the lighting specifications, proposed positioning, as well as an Iso Contour to clearly demonstrate what light overspill levels are to be expected from the site, and ensure that they do not adversely impact on existing nearby residential properties.

This can be imposed as a condition.

5.6 Environmental Health (FDC) (3/9/2020)

Having reviewed the revised information submitted, I can confirm the Environmental Health team have no further comment to make on this application.

5.7 Lead Local Flood Authority

Thank you for your consultation which we received on 1st June 2020.

We have reviewed the following documents:

Flood Risk Assessment and Drainage Strategy, Create Consulting Engineers Ltd, Ref: TT/CC/P19-1853/01, Dated: March 2020

Based on these, as Lead Local Flood Authority (LLFA) we have no objection in principle to the proposed development.

The above documents demonstrate that surface water from the proposed development can be managed through the use of permeable paving over the extent of the car parking and access areas. The permeable paving will provide storage before discharge to the watercourse to the east of the site at a rate of 1.0l/s. It will be explored whether the watercourse can be lowered slightly to allow a gravity connection, however if this is not feasible, a pumped outfall will be required.

Water quality has been adequately addressed when assessed against the Simple Index Approach outlined in the CIRIA SuDS Manual.

We request the following condition is imposed:

Condition

No above ground works shall commence until a surface water drainage scheme for the site, based on sustainable drainage principles, has been submitted to and approved in writing by the local planning authority. The scheme shall subsequently be implemented in full accordance with the approved details prior to occupation of the first dwelling.

The scheme shall be based upon the principles within the agreed Flood Risk Assessment and Drainage Strategy prepared by Create Consulting Engineers Ltd (ref: TT/CC/P19-1853/01) dated March 2020 and shall also include:

a) Full results of the proposed drainage system modelling in the 100%, 3.3% and 1% Annual Exceedance Probability storm events (as well as 1% AEP plus climate change), inclusive of all collection, conveyance, storage, flow control and disposal

elements, together with an assessment of system performance. This should include pump failure modelling representing full pump failure and 50% storage; b) Detailed drawings of the entire proposed surface water drainage system, including levels, gradients, dimensions and pipe reference numbers; c) Full details of the proposed attenuation and flow control measures; d) Details of overland flood flow routes in the event of system exceedance, with demonstration that such flows can be appropriately managed on site without increasing flood risk to occupants;

The drainage scheme must adhere to the hierarchy of drainage options as outlined in the NPPF PPG

Reason To ensure that the proposed development can be adequately drained and to ensure that there is no increased flood risk on or off site resulting from the proposed development

Informatives

IDB Consent Part or all of your proposed development area falls within the Middle Level Commissioners (MLC) catchment and/or that of Manea and Welney IDB whose consents are managed by the MLC. All increased discharges proposed to enter watercourses directly or indirectly or any works affecting watercourses or access to or along them for maintenance if the site is within the Board's district will require MLC/IDB consent. It is therefore recommended that you contact the IDB/MLC to discuss their requirements. Further information is available at: <https://middlelevel.gov.uk/>

Pollution Control Surface water and groundwater bodies are highly vulnerable to pollution and the impact of construction activities. It is essential that the risk of pollution (particularly during the construction phase) is considered and mitigated appropriately. It is important to remember that flow within the watercourse is likely to vary by season and it could be dry at certain times throughout the year. Dry watercourses should not be overlooked as these watercourses may flow or even flood following heavy rainfall.

5.8 Cambridgeshire County Council Highways (11/6/2020)

To simplify the conditions and to avoid pre-commencement conditions, can you request the applicant annotates the access with the following notations;

- Highway access crossover to be constructed to CCC Highways Construction Specification.*
- Private access road to be sealed and drained away from the public highway.*

Defer for amended plans or re-consult for highway conditions.

5.9 Cambridgeshire County Council Highways (3/7/2020)

I have no highway objections subject to the following conditions recommendations;

1.) Prior to first use, a 2.0m wide footway will be constructed along Fodder Fen Road in accordance submitted plan TP54-11 Rev D.

Reason: In the interest of satisfactory pedestrian access.

2.) Prior to first use, the vehicular access will be constructed in accordance submitted plan TP54-11 Rev D

Reason: In the interest of satisfactory vehicular access.

Informative

1.) *This development involves work to the public highway that will require the approval of the County Council as Highway Authority. It is an OFFENCE to carry out any works within the public highway, which includes a public right of way, without the permission of the Highway Authority. Please note that it is the applicants responsibility to ensure that, in addition to planning permission, any necessary consents or approvals under the Highways Act 1980 and the New Roads and Street Works Act 1991 are also obtained from the County Council. Public Utility apparatus may be affected by this proposal. Contact the appropriate utility service to reach agreement on any necessary alterations, the cost of which must be borne by the applicant.*

2.) *The applicant should note that the nature of the highway works proposed will necessitate the completion of a Short Form 278 Highway Works Agreement between the developer and the LHA prior to commencement.*

5.10 Middle Level Commissioners IDB

No comments have been made to the LPA in relation to the application.

5.11 Arboricultural Officer (FDC)

If the trees are in third party ownership, the developer must take the necessary measures to protect those trees from damage. Whilst Common Law allows for the cutting of both branches and roots that cross the property boundary, the work must not make the trees hazardous e.g. by severing roots leading to instability or decline in their health.

There is an existing grass verge at road level sloping down and away from the road and there would appear to be potential to install a minimal dig footpath using a cellular confinement system with porous tarmac as a final wearing surface.

Another option would be to approach the tree owners with a view to removing the trees constraining the development and replanting following completion of the construction phase.

5.12 Local Residents/Interested Parties

One objection has been received (from a resident of Station Road, Manea) in relation to the following:

- Out of scale with the village
- Improved railway infrastructure needed
- Potential for anti-social behaviour and issues that come with this

Four representations have been received (from residents of Wisbech Road, Fodder Fen Road and Westfield Road, Manea) in relation to the following:

- Concerns regarding encroachment of footpath on their land
- Access to their land/devaluation of property
- Option to utilise land for access to platform
- Why is the railway land the other side of the road not being used
- Scale of car park large for those that use it
- Queries regarding the speed limit, hours of access, maintenance, lighting and fees

- Request for disabled spaces located at the nearest exit point, children's/family spaces and those for larger vehicles
- whether there is a shorter route possible to minimise the distance a person needs to travel to get to the platform
- Access width should be large enough for pedestrians, wheelchair & mobility scooter access.

Comments, where they relate to planning matters will be considered in the sections below.

- 5.13 It should be noted that only the proposal being put forward can be considered, though comments from the Parish Council have been passed to the applicant. Devaluation of property and rights of access are not planning matters.

6 STATUTORY DUTY

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires a planning application to be determined in accordance with the Development Plan unless material planning considerations indicate otherwise. The Development Plan for the purposes of this application comprises the adopted Fenland Local Plan (2014).

7 POLICY FRAMEWORK

National Planning Policy Framework (NPPF)

National Planning Practice Guidance (NPPG)

National Design Guide 2019

Context – C1

Identity – I1

Movement – M3

Nature – N3

Fenland Local Plan 2014

LP1 – A Presumption in Favour of Sustainable Development

LP2 – Facilitating Health and Wellbeing of Fenland Residents

LP3 – Spatial Strategy, the Settlement Hierarchy and the Countryside

LP13 – Supporting and Managing the Impact of a Growing District

LP14 – Responding to Climate Change and Managing the Risk of Flooding in Fenland

LP15 – Facilitating the Creation of a More Sustainable Transport Network in Fenland

LP16 – Delivering and Protecting High Quality Environments across the District

LP17 – Community Safety

LP18 – The Historic Environment

LP19 – The Natural Environment

Delivering and Protecting High Quality Environments SPD 2014

DM2 – Natural Features and Landscaping Schemes

Cambridgeshire Flood and Water SPD 2016

Fenland Rail Development Strategy 2011-31

Fenland Infrastructure Delivery Plan (IDP) 2016

8 KEY ISSUES

- **Principle of Development**
- **Design considerations and visual amenity of area**
- **Residential Amenity**
- **Community safety**
- **Parking and Highways**
- **Flood Risk/Surface Water Drainage**
- **Ecology**
- **Archaeology**
- **Land ownership**

9 ASSESSMENT

Principle of Development

- 9.1 The application site is located to the north of the railway line and does not therefore adjoin the developed footprint of the village, as such it would be classed as an 'elsewhere' location within the settlement hierarchy and Policy LP3 of the Fenland Local Plan 2014.
- 9.2 Development at such a location will be restricted to that which is demonstrably essential to the effective operation of local agriculture, horticulture, forestry, outdoor recreation, transport or utility services.
- 9.3 Policies LP1, LP2 and LP15 of the Fenland Local Plan seek to encourage delivery of a more sustainable transport network. The Fenland Infrastructure Delivery Plan strives to improve rail service usage and proposes a car park near Manea station to support this aim. Chapter 9 of the NPPF 2019 seeks to promote sustainable transport and encourage the use of public transport. As such the principal of this development is supported.

Design considerations and visual amenity of area

- 9.4 The application site is considered to relate more to the countryside than the built settlement and as such it is important that this character is retained as much as possible to limit the impact. The site is bounded by trees and vegetation which it is proposed in the main to retain and enhance, a buffer also surrounds the car park, in the main for ecology purposes, however this also mitigates the impact of the development on the character of this rural location; it is proposed to condition these to ensure their retention. Full details of the materials to be used and proposed landscaping have not been provided, however these can be dealt with by way of a condition
- 9.5 The formalisation of the access and creation of a 2m wide footway would have an urbanising impact, however this would be outweighed by the public benefit of scheme and need to ensure that well designed, safe and convenient access for all is achieved.
- 9.6 There are a number of trees, including those on third party land that could be affected by the proposal. The Council's Arboricultural Officer has advised that there are design solutions to ensure that the trees, in particular those on third party land, would not be detrimentally impacted by the proposed works. Full details of the scheme are still to be agreed and this shall be informed by an appropriate tree survey to ensure adequate mitigation is incorporated.

Residential Amenity

- 9.7 There are a number of dwellings in the vicinity of the site, 2 dwellings to the east on the opposite side of Fodder Fen Road, a dwelling to the north in association with a farm, this is located approximately 20m from the site but at least 50m from the area of the development due to the required buffer. To the south of the railway line is a workplace home estate and further dwellings on Station Road.
- 9.8 There is potential for noise and disturbance from the use of the car park and the encouragement for additional commuters to use the station, however given the scale of the development this is not considered to be significantly adverse and would be mitigated to some degree by the retention of the boundary trees and vegetation.
- 9.9 Details of external lighting have not been provided and there is potential that this could have an adverse impact on residential amenity, particularly if the lighting is in use early or late in the day. For this reason it is felt necessary to impose a condition in relation to lighting, to include a full light impact assessment and ensure a suitable scheme is provided which is fit for purpose but also would not have a significant detrimental impact on surrounding dwellings.

Community Safety

- 9.10 Due to the need to retain the boundary vegetation and trees for ecology and visual amenity purposes there is limited natural surveillance of the site. Concerns have also been raised regarding the use of the site for anti-social behaviour.
- 9.11 Full details of lighting, security measures such as CCTV along with the management and maintenance have not been confirmed, hence it is necessary to impose a condition in this regard to ensure a safe environment is achieved.

Parking and Highways

- 9.12 The development proposes the formation of an access off Fodder Fen Road and 2m (max) wide footpath to link the site with the railway station platform. Within the site are 112 parking spaces.
- 9.13 The Local Highways Authority have no objections to the proposal subject to conditions ensuring the provision of the access and footpath. Full details of the footpath design have not been provided and there is potential for this to have an impact on surrounding trees, hence a condition will be imposed to ensure adequate mitigation is secured.
- 9.14 The parking spaces are 2.5m x 5m which is considered to be adequate and there is at least 6m behind each space to enable these to be successfully entered/exited. The applicant has confirmed that it is the intention to incorporate disabled spaces within the scheme.

Flood Risk/Surface Water Drainage

- 9.15 The application site falls within Flood Zone 3, the highest risk of flooding. The sequential test, which seeks to steer new development to areas at the lowest risk of flooding would be applicable to this development. The proposal is to provide car parking to serve Manea railway station and as such would need to be located in close proximity, there is no land nearby which is at lesser risk of flooding, hence the sequential test is considered to be passed.
- 9.16 The development is considered to be 'less vulnerable' within the flood risk vulnerability and flood zone compatibility table and as such is considered appropriate development, the exception test is therefore not required.

- 9.17 Flood risk mitigation measures are set out in Section 7 of submitted Flood Risk Assessment and incorporate such measures as flood warning information signs and the closure of the site if flooding is expected.
- 9.18 Surface water will be attenuated using SUDS; the Lead Local Flood Authority have no objections to the proposal subject to a condition, and advise that submitted documents demonstrate that surface water from the proposed development can be managed through the use of permeable paving over the extent of the car parking and access areas

Ecology

- 9.19 The application was accompanied by a Preliminary Ecological Appraisal which advised that there is suitable habitat for bats, breeding birds, hedgehogs, reptiles and other species. Further surveys were also required and have since been undertaken and submitted.
- 9.20 These surveys make a number of recommendations, including the retention and enhancement of the boundary vegetation and buffer zones using native species, a lighting scheme designed to reduce light spill and impact on foraging and commuting bats and the provision of bat and bird boxes and log piles for reptiles.
- 9.21 The proposed works will result in the loss of a small area of foraging/commuting habitat. The habitats present are common and widespread in the area as such impacts are not anticipated to be significant. Conditions will be imposed in relation to the retention of the existing boundary vegetation and buffer zone in addition to the recommended enhancement/compensatory measures.

Archaeology

- 9.22 A consultation response on the previous application (F/YR14/0113/F) requested a programme of investigation be undertaken due to the site being in an area of high archaeological potential.
- 9.23 The application has been accompanied by an archaeological evaluation. The archaeological fieldwork was carried out on the 19th and 20th November 2019 and no archaeological features were revealed on the development site. An email was provided by Cambridgeshire County Council to the author of the report on 13th February 2020 which advised that the report was approved and that there were no further archaeological requirements for the site.

Land Ownership

- 9.24 Concerns were raised regarding the potential for the proposed footpath to encroach on third party land. The applicant has undertaken the relevant searches and confirmed that all works can be achieved within Local Highway Authority land.

10 CONCLUSIONS

- 10.1 Policies LP1, LP2 and LP15 of the Fenland Local Plan seek to encourage delivery of a more sustainable transport network. The Fenland Infrastructure Delivery Plan strives to improve rail service usage and proposes a car park near Manea station to support this aim. Subject to the retention and enhancement of the boundary vegetation the proposal is not considered to have a significant detrimental impact on the character of the area. There are no further requirements in relation to

archaeology and it is considered that suitable drainage, landscaping, security, lighting and ecology mitigation can be achieved. As such a favourable recommendation is forthcoming.

11 RECOMMENDATION

Grant subject to the following conditions;

Conditions

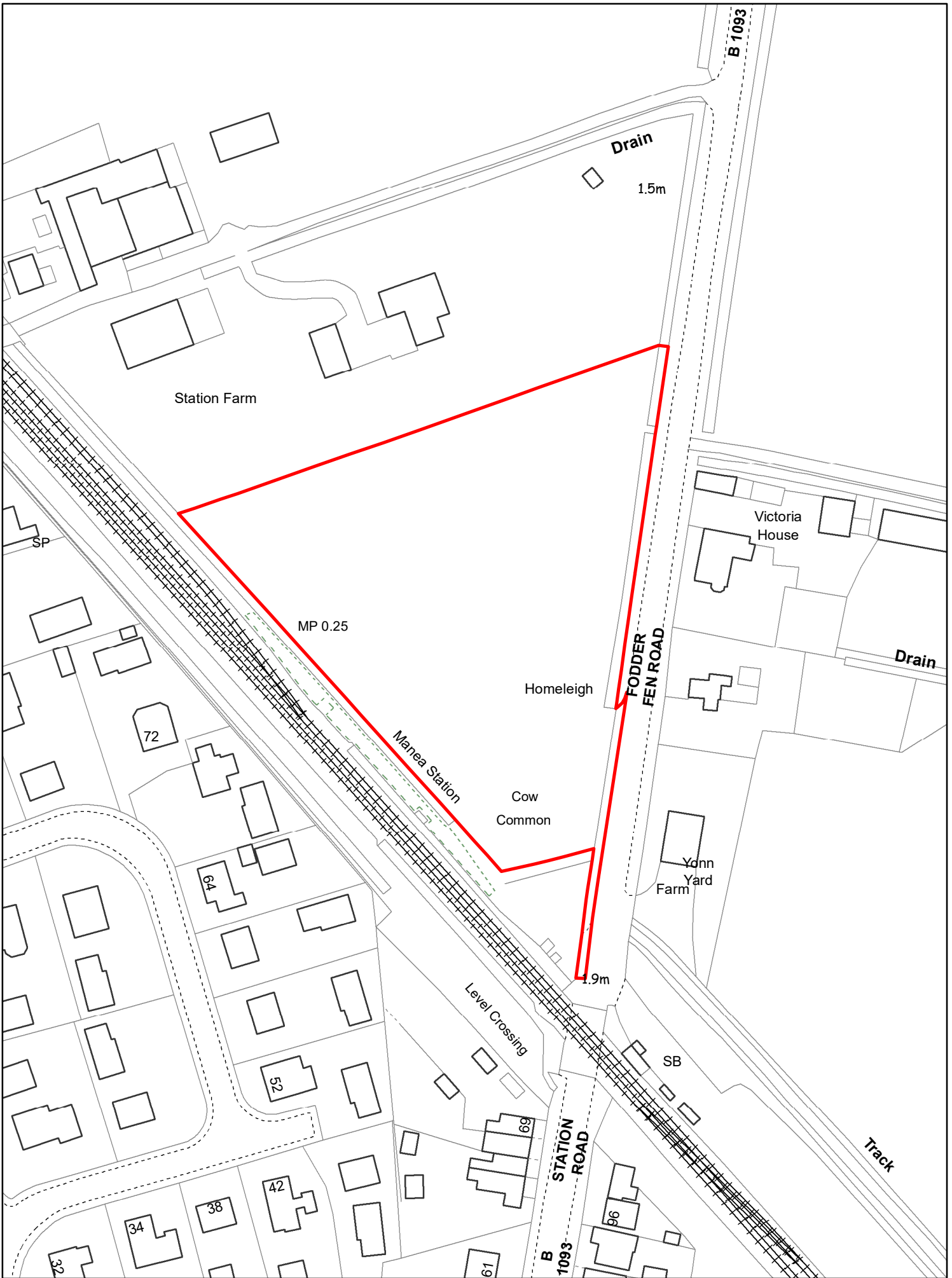
There are four pre-commencement conditions (2, 6, 9 and 13), agreement has been obtained from the applicant to impose these.

1	<p>The development permitted shall be begun before the expiration of 3 years from the date of this permission.</p> <p>Reason - To ensure compliance with Section 51 of the Planning and Compulsory Purchase Act 2004.</p>
2	<p>Prior to the commencement of the development hereby permitted, a surface water drainage scheme for the site, based on sustainable drainage principles, shall be submitted to and approved in writing by the local planning authority. The scheme shall subsequently be implemented in full accordance with the approved details prior to the first use of the development hereby approved.</p> <p>The scheme shall be based upon the principles within the agreed Flood Risk Assessment and Drainage Strategy prepared by Create Consulting Engineers Ltd (ref: TT/CC/P19-1853/01) dated March 2020 and shall also include:</p> <p>a) Full results of the proposed drainage system modelling in the 100%, 3.3% and 1% Annual Exceedance Probability storm events (as well as 1% AEP plus climate change), inclusive of all collection, conveyance, storage, flow control and disposal elements, together with an assessment of system performance. This should include pump failure modelling representing full pump failure and 50% storage; b) Detailed drawings of the entire proposed surface water drainage system, including levels, gradients, dimensions and pipe reference numbers; c) Full details of the proposed attenuation and flow control measures; d) Details of overland flood flow routes in the event of system exceedance, with demonstration that such flows can be appropriately managed on site without increasing flood risk to occupants;</p> <p>The drainage scheme must adhere to the hierarchy of drainage options as outlined in the NPPF PPG</p> <p>Reason - To ensure that the proposed development can be adequately drained and to ensure that there is no increased flood risk on or off site resulting from the proposed development, in accordance with Policy LP14 of the Fenland Local Plan 2014.</p>
3	<p>No development shall take place above slab level until a scheme for the provision of external lighting has been submitted to and approved in writing by the Local Planning Authority. Such a scheme shall be</p>

	<p>accompanied by a light impact assessment and include the lighting specifications, proposed positioning, hours of use as well as an Iso Contour to clearly demonstrate what light overspill levels are to be expected from the site. Furthermore, the recommendations of the Preliminary Ecological Appraisal 8946 V4 shall be incorporated. The approved details shall be implemented prior to the first use of the development hereby permitted and retained thereafter in perpetuity.</p> <p>Reason - In order to ensure that the site meets the crime prevention guidelines in accordance with Policy LP17, the residential amenity of surrounding dwellings is protected in accordance with Policy LP16 and ecology is not adversely impacted in accordance with Policy LP19 of the Fenland Local Plan 2014.</p>
4	<p>No development shall take place above slab level until a scheme for the hard and soft landscaping of the site has been submitted to and approved in writing by the Local Planning Authority. Subsequently, these works shall be carried out as approved. The landscaping details to be submitted shall include:-</p> <ul style="list-style-type: none"> b) means of enclosure (taking into account the recommendation of Preliminary Ecological Appraisal 8946 V4) c) car parking layout d) vehicle and pedestrian access and circulation areas e) hard surfacing, other hard landscape features and materials f) existing trees, hedges or other soft features to be retained, including measures to be taken to protect them during construction. g) planting plans, including specifications of species, sizes, planting centres number and percentage mix (taking into account the recommendation of Preliminary Ecological Appraisal 8946 V4) h) details of planting or features to be provided to enhance the value of the development for biodiversity and wildlife i) details of siting and timing of all construction activities to avoid harm to all nature conservation features <p>Should any plants or trees die, are removed or become seriously damaged or diseased, these shall be replaced in the next planting season with others of similar size and species,</p> <p>The approved hard landscaping scheme shall be carried prior to the first use of the development hereby permitted and the soft landscaping shall be carried out within the first available planting season following completion of the development or alternatively in accordance with a timetable for landscape implementation which has been approved as part of the submitted landscape scheme.</p> <p>Reason: The landscaping of this site is required in order to protect and enhance the existing visual character of the area and to reduce the</p>

	visual, environmental and ecological impacts of the development hereby permitted in accordance with Policies LP16 and LP19 of the Fenland Local Plan 2014.
5	<p>No development shall take place above slab level until a management, maintenance and security plan has been submitted to and approved by the Local Planning Authority in writing. The development shall be carried out as approved in accordance with the specified schedule contained therein.</p> <p>The plan shall include the following details:</p> <ul style="list-style-type: none"> - long term design objectives - management responsibilities - maintenance schedules - security measures, including CCTV - servicing arrangements, including refuse <p>Reason: To ensure that the site meets the crime prevention guidelines in accordance with Policy LP17 ensure that the development is adequately maintained, managed and serviced in accordance with Policy LP16 of the Fenland Plan 2014.</p>
6	<p>Prior to the commencement of the development hereby permitted, a detailed scheme in relation to a 2m (maximum) wide footway along Fodder Fen Road shall be submitted to and approved in writing by the Local Planning Authority. Such a scheme will be informed and accompanied by an appropriate arboricultural survey. The footpath shall then be constructed in accordance with the approved details prior to the first use of the development hereby permitted, and thereafter retained in perpetuity.</p> <p>Reason: In the interest of satisfactory pedestrian access and to ensure that the surrounding trees are not detrimentally impacted, in accordance with Policy LP15, LP16 and LP19 of the Fenland Local Plan 2014.</p>
7	<p>Prior to the first use of the development hereby permitted, the vehicular access shall be constructed in accordance with submitted plan TP54-11 Rev E</p> <p>Reason: In the interest of satisfactory vehicular access, in accordance with Policy LP15 of the Fenland Local Plan 2014.</p>
8	<p>Within 6-months of the commencement of the development hereby approved full details of a scheme for cycle storage shall be submitted to and approved in writing with the Local Planning Authority. The approved details shall be implemented in full prior to the first use of the development hereby permitted and thereafter retained in perpetuity.</p> <p>Reason - In the interests of security, the convenience of cyclists at the site, and to encourage sustainable forms of transport in accordance with Policies LP15 and LP16 of the Fenland Local Plan 2014.</p>

9	<p>Prior to the commencement of the development hereby approved, a pre-construction mammal walkover shall be undertaken. A report of the findings, along with any necessary mitigation and/or enhancement measures shall be submitted to and approved in writing by the Local Planning Authority. The agreed details shall then be implemented prior to the commencement of works on site and retained thereafter.</p> <p>Reason - To protect the habitats of protected species in accordance with Policy LP19 of the Local Plan.</p>
10	<p>The development hereby permitted shall be undertaken in full accordance with the recommendations (Section 4) of the Preliminary Ecological Appraisal 8946 V4.</p> <p>Reason - To protect the habitats of protected species in accordance with Policy LP19 of the Local Plan.</p>
11	<p>Prior to the commencement of above ground works in relation to the development hereby permitted, details of a range of bird nest boxes, bat boxes/tiles and log piles shall be submitted to and approved in writing by the Local Planning Authority. Such details shall include the number of boxes, designs and location within the site. The agreed details shall then be implemented prior to first use of the development hereby permitted and retained in perpetuity.</p> <p>Reason - To protect the habitats of protected species in accordance with Policy LP19 of the Local Plan.</p>
12	<p>If development has not commenced before May 2022 a repeat reptile survey shall be carried out to confirm whether reptiles are still likely to be absent from the site. Such a survey along with any necessary mitigation or enhancement measures shall be submitted to and approved in writing by the Local Planning Authority. The agreed details shall then be implemented prior to first use of the development hereby permitted and retained in perpetuity.</p> <p>Reason - To protect the habitats of protected species in accordance with Policy LP19 of the Local Plan.</p>
13	<p>Prior to the commencement of the development hereby permitted adequate temporary facilities (details of which shall have previously been submitted to and agreed in writing with the Local Planning Authority) shall be provided clear of the public highway for the parking, turning, loading and unloading of all vehicles visiting the site during the period of construction.</p> <p>Reason: To minimise interference with the free flow and safety of traffic on the adjoining public highway in accordance with Policy LP15 of the Fenland Local Plan 2014.</p>
14	Approved Plans



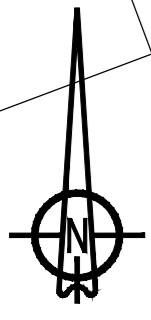
Created on: 19/08/2020

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F/YR20/0427/F

Scale = 1:1,250





Station Farm

ALL PARKING BAYS SHOWN ARE 2.5m WIDE X 5.0m LONG

OVERHEAD CABLES AND POLES

Approximate Extent of Existing Hedgeline Growth

Approximate Extent of Overgrown Land

30.0m Buffer Zone from Overgrown Area

30.0m Buffer Zone from Overgrown Area

6.0m Buffer Zone from Hedgeline

6.0m Buffer Zone from Hedgeline

TOTAL PARKING 112 SPACES

OVERHEAD CABLES AND POLES

Approximate Extent of Existing Hedgeline Growth

FODDER FEN ROAD

Homeleigh

Highway Access Crossover (See Note 3)

Private Access Road (See Note 4)

Manea Station

Yonn Yard Farm

NOTES

- All levels, chainages and dimensions are in metres, unless otherwise stated.
- This proposed car parking layout drawing has been created based upon the amalgamation of the following:-
 - A general base plan showing the land identified for car parking and its surrounding area, as taken from Ordnance Survey.
 - An approximate indication of existing features as identified from Google maps.
 - A topographical survey undertaken by Mike Worby Survey Consultancy in September 2015 of Manea Railway Station, its adjoining land and the adjacent Highway approach.
- Highway access crossover to be constructed to CCC Highways Construction Specification.
- Private access road to be sealed and drained away from the public highway.

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FENLAND DISTRICT COUNCIL, LICENCE No. 10023778, 2020

Revisions					Revisions				
Rev	By	Date	Aprvd	Description	Rev	By	Date	Aprvd	Description
					C	PR	May20		Red line boundary & new link footway adjacent to Fodder Fen Road added. Drawing title amended & "Do Not Scale" removed from Notes for Planning purposes.
E	PR	Jul20		Green shading removed from separate triangular field in SE corner and Red line boundary now shown around overall application site, to suit Planning requirements.	B	PR	Apr20		Disclaimer removed from Note 3 for Planning purposes.
					A	PR	Jan20		Available land area reduced and notional parking layout revised accordingly. Notes & hatching added.
D	PR	Jun20		Highway access crossover & private access road shown, together with new related Notes 3 & 4 to suit CCC / Planning requirement.	Designed				Eng. Chk PDR JUL '19
					Drawn	PDR	JUL '19		Approved WD JUL '19

Project
MANEA RAILWAY STATION

Title
CAR PARKING LAYOUT ON LAND ADJACENT TO THE STATION



Fenland District Council
Fenland District Council, Fenland Hall, County Road, March, Cambridgeshire, PE15 8NQ
Tel: 01354 654321 www.fenland.gov.uk

Scale **1:250 @A1** Drawing No **TP54-11** Rev. **E**

F/YR20/0531/VOC

Applicant: D Gooch
Minister Property Group Limited

Agent : Mr Jake Stentiford
Surface Planning

The College Of West Anglia, Elm High Road, Wisbech, Cambridgeshire

Variation of condition 20 to enable amendment to approved plans of planning permission F/YR16/0792/F (Erection of 137 dwellings, alterations to Ramnoth Road and Elm High Road junction to form a new vehicular & pedestrian access and associated works & infrastructure)

Officer recommendation: Grant

Reason for Committee: Called in by Councillor Hoy

1 EXECUTIVE SUMMARY

- 1.1 This submission comes forward under S73 of the Town and Country Planning Act and seeks to agree a minor-material change to the development approved under F/YR16/0792/F.
- 1.2 The principle of development has been accepted by virtue of the earlier grant of planning permission. It is therefore only the implications of the amended layout that may be revisited at this time.
- 1.3 The development proposes the same quantum of units overall, albeit the mix and design of the housing has been revisited; as has the layout of the site.
- 1.4 Although the revised design does have implications for existing residential amenity that could be deemed less acceptable to adjoining residents, i.e. where bungalows were previously proposed two storey units have been introduced, these changes whilst less desirable from the residents perspective could not be deemed to represent significant harm when considered from a planning perspective as appropriate separation distances are achieved.
- 1.5 There are no site constraints that would render the development unacceptable and as such a positive recommendation is forthcoming.
- 1.6 With regard to the S106 obligation in place it is noted that provision is included within the original agreement to supplement plans agreed under the S73 process without a new S106 having to be entered into as such there is no requirement to revisit the terms of the S106.
- 1.7 It should be acknowledged that it is intended to deliver the scheme as a 100% affordable housing which although having implications for the S106 contributions attracted by the development does represent a significant contribution to affordable housing provision within the district which is welcomed.

2 SITE DESCRIPTION

- 2.1 The site was the former agricultural campus of the College of West Anglia and previously comprised a number of buildings which were demolished in 2016, currently the site is vacant and overgrown.
- 2.2 The site is a rectangular area of land covering some 5.6ha and measures some 516 m in length by 130 m in depth and it is located about 1.0km to the south east of Wisbech Town Centre. It is bounded by established residential areas along its northern and southern boundaries. The residential development along Westmead Avenue to the south of the application site comprises a mixture of semi-detached and detached dwellings set in large plots along a linear tree lined avenue.
- 2.3 The existing development along the northern boundary of the application site comprises of a row of bungalows set in a linear fashion along Falklands Drive. This development is denser, more compact and with open frontages and set on smaller plots than the development along Westmead Avenue.
- 2.4 Ramnoth Road/Elm High Road forms the western boundary of the site, and Meadowgate Lane its eastern boundary. Watercourses run along the northern, southern and western boundaries of the site. The site is generally level with established boundary planting.
- 2.5 Vehicular access to the site is currently off Elm High Road, and there is a pedestrian access off Meadowgate Lane.
- 2.6 The site is within Flood Zone 1.

3 PROPOSAL

- 3.1 Planning permission was granted for a scheme of 137 dwellings on this site in March 2020 following completion of the related S106 agreement.
- 3.2 This variation of condition application seeks to amend the approved layout to meet the requirements of the current applicant who wishes to deliver the entire site, i.e. 137 dwellings as affordable housing.
- 3.3 As indicated in the agents covering letter '*The proposals entail an overall amendment to the approved layout which increases the density of the plots toward the western end of the site, in accordance with the requirements of the Registered Provider, whilst maintaining the overall form of the approved scheme, so that a larger area of Public Open Space is provided at the eastern end, occupying the area currently occupied by the residential block comprising Plots 115-119 & 122-126.*'
- 3.4 It should also be noted that as a consequence of the new developers requirements the house types designs has been revisited and the overall mix changed as follows:

House types	Original Scheme	Proposed scheme
1 bed flats	6 (House Type Z - a three- storey block)	8 (House Type E - constructed in 2 x 2-storey blocks each having 4 units)
2 bed flat	6 (House Type Z - a three-storey block)	N/a
2 bed single-storey	9 (House Type A)	6 (House Type F)

2 bed two storey	43 (House types B & C)	48 (House Types A & B)
3 bed single-storey	-	5 (House Type G)
3 bed two-storey	73 (House types D, E, J & W)	68 (House Type C)
4 bed two-storey	-	2 (House Type D)

- 3.5 Full plans and associated documents for this application can be found at:
<https://www.fenland.gov.uk/publicaccess/applicationDetails.do?keyVal=QC0S1TH E01U00&activeTab=summary>

4 SITE PLANNING HISTORY

F/YR16/0792/F	Erection of 137 dwellings, alterations to Ramnoth Road and Elm High Road junction to form a new vehicular & pedestrian access and associated works & infrastructure	Granted 23.03.2020
F/YR16/0319/SC	Screening Opinion: Residential development (139 dwellings max) with associated landscaping	Further details not required 17.06.2016.
F/YR15/0710/DE1	Demolition of existing buildings	Further details not required 21.09.2015.
F/YR08/0584/O	Residential development (5.58 ha) involving demolition of all existing buildings	Resolution to approve subject to S106 agreement.
F/YR07/1328/O	Residential development (5.58) involving demolition of all existing buildings	Refused* 14.05.2008

* Contrary to Officer and Highway Authority recommendation on the grounds that the access was inadequate. Appeal withdrawn.

5 CONSULTATIONS

- 5.1 **Wisbech Town Council:** Recommend that the application be supported
- 5.2 **Ward Councillor (Councillor S Hoy):** *'A number of residents have contacted me about this. The change from bungalows to houses will cause serious overlooking. As such I need to call this in'.*
- 5.3 **Kings Lynn & West Norfolk Borough Council:** *'Please be advised that the BC of KLWN have no objections to the proposal, subject to there being no adverse impact upon the Wisbech Access Strategy'.*
- 5.4 **Highways England:** *'Referring to the planning application referenced above, dated 26 June 2020, application for the variation of condition 20 to enable amendment to approved plans of planning permission F/YR16/0792/F, College of*

West Anglia Elm High Road, Wisbech, notice is hereby given that Highways England's formal recommendation is that we offer no objection. [...] The revised condition does not material affect the operation of the A47 trunk road. Consequently, we offer no objection'.

- 5.5 CCC Transport Assessment team:** *'Background: The document reviewed is the Revised Planning Layout and Masterplan relating to a the proposed variation of Condition 20 (Approved Plans) in respect of a proposed development of 137 dwellings on the former College of West Anglia site off Ramnoth Road in Wisbech. The Transport Assessment Team have previously made comments in respect of the proposed improvements to the access of Ramnoth Road with Elm High Road and made appropriate recommendations.*

Comments: The submitted information shows that the site access is to be located in the same position as previously proposed. This will not therefore impact on the proposed junction works to Ramnoth Road/Elm High Road being brought forward as part of the Wisbech Access Strategy.

Conclusion: The proposed alterations to the layout are acceptable to the Transport Assessment Team who therefore raise no objections. The Development Management Team will make further specific comments in respect of the layout.

- 5.6 Cambridgeshire County Council Highways Authority:** *'Can you request the applicant provides a fully dimensioned site plan with carriageway and footway widths, horizontal curvature and kerb radii. I assume the only part of the development layout that has been affected is the eastern side due to the drainage features. Can you request tracking plans for the turning heads? They appear excessively large. They should be reduced to the minimum required for the turning space of a 11.5m refuse vehicle.'*

- 5.7 Designing Out Crime Officers:** *I am happy to support this proposal as I consider that community safety and vulnerability to crime should be addressed. This layout affords good natural surveillance across all areas including proposed open spaces which has footpaths that are direct, with no obvious dark and hidden spaces. This helps promote a safe environment and encourage use by pedestrians, cyclists etc, and again further improving surveillance which should encourage community engagement and reduce distraction offences, protecting elderly/vulnerable residents. Parking appears in the main to be in curtilage to owners' homes. As the proposal is for all the homes to be affordable I would like to see a lighting plan when available and would ask for column lighting in all areas not just adopted roads. I would also ask for consideration that a Secured by Design application be considered as I would like to see this development receive the higher Gold accreditation.*

- 5.8 Housing Strategy (FDC):** *I have looked at this variation of condition application and I am aware that this scheme will now be coming forward as a 100% affordable housing scheme. I can see from the application that a new housing mix has been proposed below:*

*8 x 1 bed maisonettes
6 x 2 bed bungalows
48 x 2 bed houses
5 x 3 bed bungalows
68 x 3 bed houses*

2 x 4 bed houses

I am happy to see the proposal of bungalows in the mix, as we seldom get new 2 and 3 bedroom bungalows coming forward. These dwellings are ideal for applicants and families on the housing waiting list who are unable to live in multi-storey housing. I support this application and the above property mix. The tenure mix of these units has yet to be agreed; however these discussions are ongoing with the Registered Provider.

5.9 CCC (Lead Local Flood Authority): *The proposals involve amendments to the approved layout by increasing the density of housing to the west of the site. The submitted Drainage Strategy Report demonstrates that surface water will continue to be managed in line with the previously agreed strategy. This strategy involves managing surface water through the use of an attenuation basin and a complex flow control, restricting surface water to a rate of 14.2 l/s, 20.2 l/s and 24.8 l/s for the 1 in 1, 1 in 30 and 1 in 100 (plus 40% climate change) year storm events before it outfalls into the IDB drain to the east of the site. We therefore have no objection to the application. Recommends informatives with regard to the need to secure IDB Consent and with regard to pollution control.*

5.10 Kings Lynn Drainage Board: *'The site is within the Internal Drainage District (IDD) of the King's Lynn Internal Drainage Board (IDB) and therefore the Board's Byelaws apply. [...] The adoption of a watercourse is an acknowledgement by the Board that the watercourse is of arterial importance to the IDD and as such will normally receive maintenance from the IDB. In order to avoid conflict between the planning process and the Board's regulatory regime and consenting process please be aware of the following:*

- *I note that the applicant intends to discharge surface water to a watercourse. The proposed development was granted land drainage consent in line with the Board's byelaws (specifically byelaw 3) on 3rd May 2018. This consent is for the discharge of surface water from 25,580 m² impermeable surface, to be limited to a maximum rate of 24.8 litres per second under all circumstances. Should there be any changes to the impermeable area or rate of discharge, the applicant should contact this office to discuss any proposed alterations to the agreed conditions.*
- *Should the applicant's proposals include works within 9 metres of the Board adopted watercourse adjacent to the site's eastern boundary (DRN145P1101 College Drain) consent would be required to relax Byelaw 10 (no works within 9 metres of the edge of drainage or flood risk management infrastructure). I cannot see that consent has been granted for the placement of an outfall pipe across the 9m maintenance strip to outfall into said watercourse*

Whilst the consenting process as set out under the Land Drainage Act 1991 and the aforementioned Byelaws are separate from planning, the ability to implement a planning permission may be dependent on the granting of these consents. As such I strongly recommend that the required consent is prior to the determination of the planning application.'

5.11 Local Residents/Interested Parties: Six letters of objection (from residents in Falklands Drive who have boundaries that address proposed Plots 120-123) have been received, with three of these from the same household, along with a letter of representation from a resident of Ramnoth Road; these may be summarised as follows:

- Design/Appearance
- Does not comply with Policy LP16
- Overlooking/loss of privacy
- Loss of view/outlook
- Proximity to property
- Visual impact
- Wildlife concerns
- *The layout and siting of 2 storey houses along the Northern boundary is unsympathetic to the residents of Falklands Drive which are predominantly bungalows and therefore affects visual amenity.*
- *Plots 122 and 123 (2 storey houses) will directly overlook our bungalow, they are orientated so that the windows look straight into our back windows and entire rear garden; this will lead to a loss of privacy and will certainly impact on the enjoyment of our home and garden.*
- *The plans that were approved on 23rd March 2020 (F/YR16/0792/F) were for single storey properties; plots 52, 53 and 54, directly behind our home, with which we were content, as there was no issue of overlooking and loss of our privacy.*
- *Currently enjoy total privacy to the rear of our bungalow and the proposed changes to the plans now mean that, instead of a single storey dwelling, a 2 storey property will be positioned, so that it directly looks into our bedroom, conservatory and onto our patio.*
- *A screen of approx. 15ft would be necessary to mitigate a direct line of sight from the proposed property's first floor windows. If we let our existing conifer hedge grow to this height, to afford us the privacy that we currently have, it would be totally unmanageable.*
- *Concerns about the proposed pedestrian footpath between plots 121 & 122 and behind 121. This will run along behind existing properties on Falklands Drive. It will presumably be fenced on both sides, meaning an area for the potential of loitering and anti-social behaviour.*
- *Although the Submission letter [...] states: "The house types now proposed reflect the design principles of those approved, whilst again ensuring that the requirements for affordable housing are met. It is considered that 'The following plots do not: plot 52,53,54 (approved) - 1 Storey - plot 122 & 123 (proposed) - 2 storey'*
- *'We ask that this variation to condition 20 is refused and that the developers build the approved single storey properties that are less intrusive to neighbouring bungalows.'*
- *'the original plan was for Bungalows, which I have no problem with at all, the new application with Houses will take away the privacy of my home and several of us along Falklands Drive, upstairs Windows overlooking my Garden is an invasion of privacy, and also looking directly into my 2 Bedrooms that are at the rear of the Bungalow I live in. Can consideration be taken into us existing residence when decisions are being made'.*
- *Object to the new plans put in place for 2 storey houses being built behind mine and others bungalows. 18-22 Falklands drive and onwards! We was happy for the plans to go ahead with the bungalows being built, but definitely NOT 2 storey houses! This will have a huge impact on our privacy in our homes and gardens, and will also decrease the value of our homes!*
- *Queries why they have not been consulted - officers have subsequently clarified with the writer that they did not have an adjoining boundary with the site and therefore fell outside the adopted consultation guidelines.*

Following on from a site visit undertaken by the Case Officer and further communication from the agent one of the earlier contributors has written again to express their concerns regarding the scheme and the onward process.

- *'The relationship between existing and proposed dwellings are in no way similar and are much worse for the north western corner of the site affecting Residential Amenity significantly. The proposed change in this corner, is for 2 storey houses behind a row of bungalows on Falklands Drive. This will cause serious overlooking, the presence of houses will be completely dominating and in the case of plots 122 & 123 will cause an entire loss of privacy as they directly face into our properties.*
- *There have been three objections from residents concerning this and not just one as Jake's email suggests, and in any case, we are extremely disappointed that it is felt that our circumstances should not be taken into account.*
- *It is our understanding that it is not the volume of local opposition but the legitimacy and content of an objection in relation to material planning issues.*
- *We understand from [the site visit], that because there is a distance of 29m wall to wall, our objection is not considered 'significant'. Whilst we appreciate there are standards and procedures to follow, we would ask that our objection is not dismissed on these grounds and that the real impact on our lives and enjoyment of our home is taken into account. No privacy at all, direct overlooking and whilst we have not complained about view (as this is not a material consideration), the fact that we will now be looking at 16 second storey windows from the rear of our home, instead of roofs of bungalows, which feels very intrusive.*
- *It does seem unfair and unjust that because all properties are now to be affordable housing, it should make any difference to our views being looked into at committee. On the approved plans, there are 9 bungalows, 6 of which are in the north western corner (which received no objections!) On the proposed plans, there are 11 bungalows, none of which have been left in this corner. Can we ask why have they been spaced out, many running behind Westmead avenue and could not be left where they have been approved?*

We would ask again that our concerns are addressed further and consideration given to the points we have re-iterated.'

6 STATUTORY DUTY

- 6.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires a planning application to be determined in accordance with the Development Plan unless material planning considerations indicate otherwise. The Development Plan for the purposes of this application comprises the adopted Fenland Local Plan (2014).

7 POLICY FRAMEWORK

7.1 National Planning Policy Framework (NPPF) July 2018

Paragraph 2: Planning law requires that applications for planning permission must be determined in accordance with the development plan.

Paragraph 11: Presumption in favour of sustainable development.

Paragraph 47: Determine applications in accordance with development plan

Paragraphs 54-56: Planning Conditions and Obligations

7.2 National Planning Policy Guidance (NPPG)

7.3 **National Design Guide**

Context: C1 - Relationship with local and wider context

Identity: I1 - Respond to existing local character and identity

Built Form B2 - Appropriate building types and forms

Movement: M3 - Well-considered parking, servicing and utilities infrastructure for all users

Homes and Buildings: H1 - Healthy, comfortable and safe internal and external environment

7.4 **Fenland Local Plan 2014**

LP1 – A Presumption in Favour of Sustainable Development

LP2 – Facilitating Health and Wellbeing of Fenland Residents

LP5 – Meeting Housing Need

LP14 – Responding to Climate Change and Managing the Risk of Flooding in Fenland

LP15 – Facilitating the Creation of a More Sustainable Transport Network in Fenland

LP16 – Delivering and Protecting High Quality Environments across the District

LP17 – Community Safety

8 **KEY ISSUES**

- **Principle of Development**
- **Implications of changes - Residential amenity and character**
- **Implications of changes - Highways, Access and Access Improvement scheme**
- **Implications of changes - Drainage and flood risk**
- **Other matters**
- **S106 Implications**

9 **ASSESSMENT**

Principle of Development

- 9.1 The principle of development has been accepted by virtue of the earlier grant of planning permission. It is therefore only the implications of the amended layout that may be revisited at this time. These considerations are restricted to whether the scheme will impact on the locality in terms of character and appearance and whether there are any residential amenity impacts arising as a result of the proposal as a consequence of the revised layout.
- 9.2 Making a judgement on “materiality” in any particular case is one of fact and degree, along with taking into account the likely impact of the amendment on the local environment. Materiality needs to be assessed by considering the development as a whole, not just part of it. The basis for forming a judgement on materiality is always the original planning permission. There is not a set of prescriptive rules to what is or is a minor material change, as each case is different and considered on its individual merit.

Implications of changes - Residential amenity and character

- 9.3 As indicated in the proposal section the quantum of development remains as per the original approval with 137 dwellings being provided on the site, albeit their repositioning will see an increased density with a larger area of public open space being delivered to the western section of the site as a consequence.

- 9.4 It is appreciated that a number of residents in Falklands Drive have raised objection to the scheme as it is now proposed to locate two-storey houses to the south of their dwellings as opposed to the bungalows originally planned in this location. However the planning assessment of the scheme must be restricted to whether the changes proposed are acceptable in residential amenity terms in their own right, not as to whether they are any 'less' acceptable than an earlier approval.
- 9.5 In this regard it is noted that separation distances in respect of wall to wall distances range from 24 metres at the closest points with the majority of separation distances being 29 metres or in excess of this. Whilst there are no adopted standards with regard to separation distances within the FLP a usual benchmark of 22 metres is adopted nationwide. As such it is clear that appropriate levels of separation are achieved with regard to properties situated in Falklands Drive that share a common boundary with the site. It is acknowledged that there will be some loss of privacy and overlooking as a result of this development however this is not considered so significant as to render the scheme unacceptable.
- 9.6 With regard to separation distances between the development and existing dwellings in Westmead Avenue again there are no issues with regard to proximity nor are there any significant consequences with regard to residential amenity.
- 9.7 Comments made regarding the provision of a footway between Plots 121 and 122 are noted however it is clear from the drawings that this footway is purely to enable access to the rear garden of Plot 120 and it therefore not perceived to represent any issues in terms of anti-social behaviour, such arrangements are often common place within residential areas and indeed similar occurrences are shown elsewhere within the development.

Implications of changes - Highways, Access and Access Improvement scheme

- 9.8 The scheme details supplement the approved layout within the site and have no consequence for the Wisbech Access Improvements; this having been confirmed by the Transport Assessment Team at Cambridgeshire County Council.
- 9.9 In essence the earlier scheme approval accepted a default position where the developer would be responsible for the access and junction upgrades as proposed under this submission with a proviso for a capital contribution in lieu if the works proposed under the WAI scheme were to be commenced earlier. Given that the timescales for the delivery of the Strategy works are unknown, and the time limit for planning permission is three years the default position was considered at the time, and remains, reasonable and acceptable. Furthermore, the applicant has previously demonstrated that the application works will not prejudice the implementation of the Strategy proposals and given that no changes are proposed with regard to this aspect of the current scheme there are no matters to address with regard to this aspect of the proposal.
- 9.10 Comments made regarding the internal road layout, i.e. tracking for the turning heads and the request for fully dimensioned plans have been relayed to the agent and the details subsequently received and are currently subject to reconsultation; the formal consultation response of CCC highways will be reported to the committee by way of written update. However the scheme largely follows the earlier approved details in terms of the internal road layout and this is a drafting

issue rather than one of principle. Subject to this detail being agreed the scheme will continue to comply with Policy LP15 of the FLP (2014). Should this information not be agreed at the time of decision a safeguarding condition is included below at No. 15 to facilitate the agreement of such details (such a condition having been imposed on the original approval as granted).

Implications of changes - Drainage and flood risk

- 9.11 As indicated in the consultation response of the LLFA the layout changes have required the submission of a revised drainage strategy for the site, this revised strategy having been accepted.
- 9.12 The comments of the Kings Lynn Drainage Board are noted and whilst ideally the IDB consent would be in place prior to the determination of the planning application this could not be considered reasonable in planning terms; it will be for the developer to ensure that they have the necessary consents in place prior to the commencement of development. Should any matters arise relating to the drainage strategy which necessitate it being revisited there are mechanisms within the planning regime for this to occur.

Implications of changes - Housing delivery

- 9.13 The revised scheme will see the delivery of 137 affordable housing units which will be a significant contribution to the needs of the District.

Other matters

- 9.14 The comments of the Designing Out Crime Officer are noted in so far as they relate to the provision of external lighting, however noting that the earlier consent as issued was silent with regard to this matter such a requirement may not be introduced at this juncture. Notwithstanding this the detailed scheme relating to the estate roads will make provision for lighting as an integral part of the same and it will be for the County Council to secure relevant details at design stage.

S106 Implications

- 9.15 An application under Section 73 results in the issue of a new stand-alone planning permission, as such all relevant conditions and legal obligations must go forward to the new consent. It is noted that the Section 106 which was entered into with regard to the earlier consent makes provision for the substitution of plans and other variations at Para 14, with Para 14.2 identifying that in the case of an application made under Section 73 there will be no requirement to enter into a supplemental deed. Noting that:

'If the parties shall so agree in writing in relation to any planning permission granted as a result of any application under section 73 of the Act affecting the Permission the parties shall comply with the terms of this Agreement as if the definition of the Permission in this Agreement had been replaced by the description of the said planning permission granted as a result of any application under section 73 of the Act affecting the Permission with the intention that the provisions of this Agreement were the new planning permission granted as a result of such application and a memorandum of that agreement shall be endorsed on the face of the Agreement which is recorded on the planning register.'

10 CONCLUSIONS

- 10.1 The layout changes proposed have been fully assessed and are deemed to be minor material amendments in the context of the approved scheme. The development comprises the same quantum of development and whilst the layout and dwellings are at variance to the original approval the changes are not so significant as to require the scheme to be considered as a new application.
- 10.2 It is accepted that there will be residential amenity impacts arising from the revised proposals, over and above those which would have occurred should the initial scheme have been delivered, however these impacts are no so significant as to render the scheme unacceptable.
- 10.3 Furthermore the delivery of the development exclusively as affordable housing is welcomed.

11 **RECOMMENDATION: Grant** subject to the following conditions:

Conditions

1	<p>The development permitted shall be begun before 22nd March 2023.</p> <p>Reason - To ensure compliance with Section 51 of the Planning and Compulsory Purchase Act 2004.</p>
2	<p>Development above slab level shall not begin until full details of the materials to be used for the external walls and roof are submitted to and approved in writing by the Local Planning Authority. The development shall then be carried out in accordance with the approved particulars and retained in perpetuity thereafter.</p> <p>Reason - To safeguard the visual amenities of the area in accordance with Policy LP16 of the Fenland Local Plan, 2014.</p>
3	<p>Development above slab level shall not begin until full details of both hard and soft landscape works are submitted to and approved in writing by the Local Planning Authority. Subsequently, these works shall be carried out as approved. The landscaping details to be submitted shall include:-</p> <ul style="list-style-type: none"> a) proposed finished levels [earthworks to be carried out] b) means of enclosure c) car parking layout d) vehicle and pedestrian access and circulation areas e) hard surfacing, other hard landscape features and materials f) existing trees, hedges or other soft features to be retained g) planting plans, including specifications of native species, sizes, planting centres number and percentage mix. h) details of planting or features to be provided to enhance the value of the development for biodiversity and wildlife i) details of siting and timing of all construction activities to avoid harm to all nature conservation features j) location of service runs k) management and maintenance details <p>Reason - The landscaping of this site is required in order to protect and enhance the existing visual character of the area and to reduce the visual and environmental impacts of the development hereby permitted in accordance with</p>

	Policy LP16 of the Fenland Local Plan, 2014.
4	<p>All hard and soft landscape works including any management and maintenance plan details, shall be carried out in accordance with the approved details. All planting seeding or turfing and soil preparation comprised in the above details of landscaping shall be carried out in the first planting and seeding seasons following the occupation of the buildings, the completion of the development, or in agreed phases whichever is the sooner, and any plants which within a period of five years from the completion of the development die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species, unless the local planning authority gives written consent to any variation. All landscape works shall be carried out in accordance with the guidance contained in British Standards, unless otherwise agreed in writing by the Local Planning Authority.</p> <p>Reason - To ensure proper implementation of the agreed landscape details in the interest of the amenity value of the development in accordance with Policy LP16 of the Fenland Local Plan, 2014.</p>
5	<p>Notwithstanding the provisions of the Town & Country Planning (General Permitted Development) (England) Order 2015, (or any Order or Statutory Instrument revoking and re-enacting that Order with or without modification), planning permission shall be required for the following developments or alterations:</p> <ol style="list-style-type: none"> i) the erection of freestanding curtilage buildings or structures including car ports, garages, sheds, greenhouses, pergolas, or raised decks (as detailed in Schedule 2, Part 1, Classes A and E); ii) the erection of house extensions including conservatories, garages, car ports or porches (as detailed in Schedule 2, Part 1, Classes A and D); iii) alterations including the installation of additional windows or doors, including dormer windows or roof windows (as detailed in Schedule 2, Part 1, Classes A and B); iv) alterations to the roof of the dwellinghouse (as detailed in Schedule 2, Part 1, Class C); iv) the erection of any walls, fences or other means of enclosure to all boundaries/the boundary of the site (as detailed in Schedule 2, Part 2, Class A). <p>Reason - To prevent overlooking of neighbouring properties, in the interest of the protection of residential amenity and to safeguard the visual amenities of the area to accord with Policy LP 16 of the Fenland Local Plan (2014).</p>
6	<p>No occupation or use of the development shall take place until a Travel Plan has been submitted to and approved by the Local Planning Authority. The Travel Plan shall be implemented before commencement of first use of the development hereby approved and in line with provisions and timescales set out with the Travel Plan and shall include the following:</p> <ul style="list-style-type: none"> - The identification of targets for trip reduction and modal shift; - The methods to be employed to meet these targets; and - The mechanisms for monitoring and review; - The mechanisms for reporting; - The penalties to be applied in the event that targets are not met; - The mechanisms for mitigation; - Implementation of the travel plan to an agreed timescale of timetable and its operation thereafter;

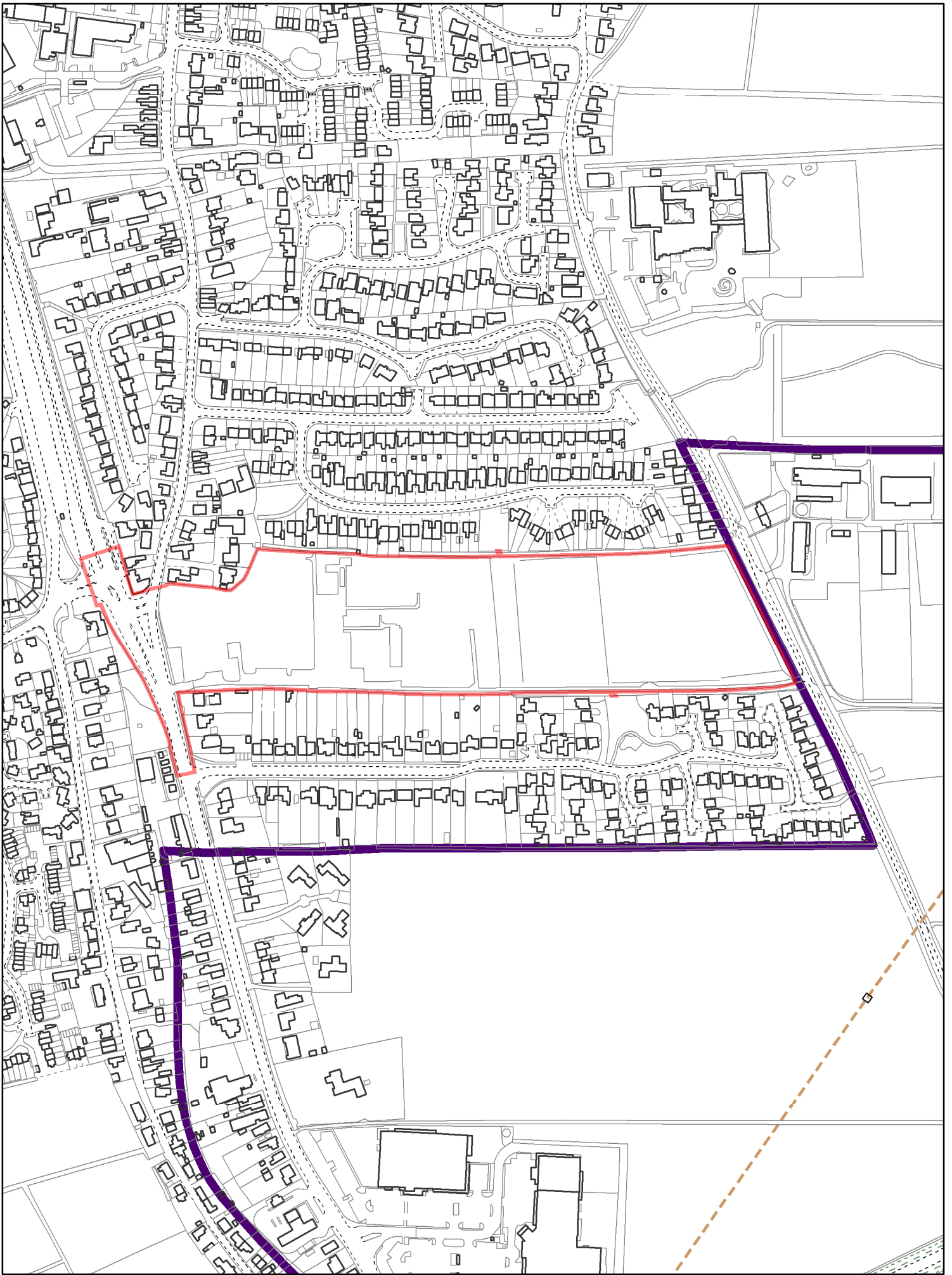
	<p>- Mechanisms to secure variations to the Travel Plan following monitoring and reviews.</p> <p>The completed development shall be occupied in accordance with the approved Travel Plan which shall be retained in place thereafter unless otherwise amended in accordance with a review to be agreed in writing by the Local Planning Authority.</p> <p>Reason - To provide sustainable transport measures for visitors and staff and to ensure that the impact of the proposal on the free and safe flow of traffic on the highway is kept to a minimum to accord with Policy LP15 of the Fenland Local Plan.</p>
7	<p>Development above slab level shall not begin until the following details shall be submitted to and agreed in writing by the local planning authority, and the development shall thereafter be carried out in accordance with the approved details.</p> <p>a) All external lighting proposals to be baffled downwards and pointed away from retained areas of woodland and trees. b) Details of integral bat boxes and bat tiles, and bird boxes. c) Overnight trench protection for hedgehogs</p> <p>Reason - In the interests of biodiversity enhancement and mitigation of the impact of the development in accordance with Policy LP19 of the Fenland Local Plan.</p>
8	<p>If, during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the Local Planning Authority) shall be carried out until the developer has submitted, and obtained written approval from the Local Planning Authority for, and amendment to the remediation strategy detailing how this unsuspected contamination shall be dealt with. The development shall then be carried out in full accordance with the amended remediation strategy.</p> <p>Reason - To control pollution of land and controlled waters in the interests of the environment and public safety to accord with Policy LP16 of the Fenland Local Plan (2014).</p>
9	<p>Prior to the commencement of development a Construction Management Plan shall be submitted to and approved in writing by the Local Planning Authority.</p> <p>The Construction Management Plan shall include the following:-</p> <ul style="list-style-type: none"> - Hours of working - Parking, Turning and Loading/Unloading areas for all construction/contractors vehicles - Site compounds/storage areas - Details of wheel cleaning or road cleaning equipment; - A noise management plan including a scheme for the monitoring of construction noise; - A scheme for the control of dust arising from building and site works - Details of remedial measures to be taken if complaints arise during the construction period. <p>The development shall thereafter take place in accordance with the approved Construction Management Plan.</p>

	Reason - In the interests of the amenity of the area and highway safety, in accordance with policies LP15 and LP16 of the Fenland Local Plan 2014.
10	<p>Prior to the first occupation of the development hereby approved, a scheme for the provision of fire hydrants or equivalent emergency water supply shall be submitted to and approved in writing by the Local Planning Authority. The approved details shall be implemented and made available for use prior to the occupation of the first dwelling.</p> <p>Reason - In the interests of the safety of the occupiers and to ensure there are available public water mains in the area to provide for a suitable water supply in accordance with infrastructure requirements within Policy LP13 of the Fenland Local Plan 2014.</p>
11	<p>Development above slab level shall not begin until a detailed surface water drainage scheme for the site, based on Drainage Strategy Report, Stirling Maynard, P20047-SMCE-ZZ-XX-RP-D-0001. Dated: 24th June 2020, has been submitted to and approved in writing by the Local Planning Authority. The scheme shall subsequently be implemented in full accordance with the approved details before the development is completed.</p> <p>Reason - To prevent the increased risk of flooding, to improve and protect water quality, and improve habitat and amenity.</p>
12	<p>Details for the long term maintenance arrangements for the surface water drainage system (including all SuDs features) to be submitted and approved in writing by the Local Planning Authority prior to the first occupation of any of the dwellings hereby permitted. The permitted details should identify runoff sub-catchments, SuDs components, control structures, flow routes and outfalls. In addition, the plan must clarify the access that is required to each surface water management component for maintenance purposes. The maintenance plan shall be carried out in full thereafter.</p> <p>Reason - To ensure the satisfactory maintenance of un-adopted drainage systems in accordance with the requirements of paragraphs 103 and 109 of the National Planning Policy Framework.</p>
13	<p>Prior to the first occupation of any dwelling the road(s), footway(s) and cycleway(s) required to access that dwelling shall be constructed to at least binder course surfacing level from the dwelling to the adjoining County road.</p> <p>Reason - To ensure a satisfactory means of access in accordance with policies LP15 and LP16 of the Fenland Local Plan.</p>
14	<p>No part of the development shall be occupied until details of the proposed arrangements for future management and maintenance of the proposed estate roads and private drives within the development have been submitted to and approved by the local planning authority. The estate roads and private drives shall thereafter be maintained in accordance with the approved management and maintenance details until such time as an agreement has been entered into under section 38 of the Highways Act 1980 or a private management and maintenance company has been established.</p> <p>Reason - In the interests of highway safety and to ensure compliance with Policies LP15 and LP16 of the Fenland Local Plan, adopted May 2014</p>
15	Notwithstanding the submitted plans development above slab level shall not commence until details of the turning head geometry, layout and construction shall be submitted and approved by the Local Planning Authority. Prior to first occupation the turning heads shall then be constructed in strict accordance with

	<p>the approved plans.</p> <p>Reason - To provide adequate turning facilities in the interests of highway safety to accord with Policy LP15 of the Fenland Local Plan (2014).</p>
16	<p>Prior to the first occupation of any dwelling the parking areas shown on the approved plans for that dwelling shall be laid out and thereafter retained for that specific use.</p> <p>Reason - To ensure the permanent availability of the parking / manoeuvring area, in the interests of highway safety to accord with Policy LP15 of the Fenland Local Plan (2014).</p>
17	<p>Prior to first occupation of the development hereby approved, visibility splays shall be provided as shown on the approved plans (COWA-BWB-01- DR-TR101 and 4905/SK06A) and shall be maintained thereafter free from any obstruction exceeding 0.6m above the level of the adjacent highway carriageway.</p> <p>Reason - In the interests of highway safety to accord with Policy LP15 of the Fenland Local Plan (2014).</p>
18	<p>Prior to first occupation, Ramnoth Road shall be realigned to form a new zebra crossing and new signalised junction with Elm High Road, laid out in accordance with submitted plan COWA-BWB-GEN-01-DR-TR-101 revision P3 and constructed in accordance with detailed engineering plans to be submitted and approved in writing by the Local Planning Authority unless otherwise agreed in writing. Submitted details shall include full geometry and alignment details for new footway infrastructure linking the zebra crossing and signalised junction with the existing footway network along Ramnoth Road and Elm High Road.</p> <p>Reason - In the interests of satisfactory pedestrian and vehicular access to accord with Policy LP15 of the Fenland Local Plan (2014).</p>
19	<p>The development hereby permitted shall be carried out in accordance with the following approved plans and documents</p> <p>20014 0100-P02 SITE LOCATION PLAN 20014 0101-P02 EXISTING SITE LAYOUT 20014 0102-P07 PROPOSED SITE LAYOUT 20014 0103-P05 MATERIAL DISTRIBUTION PLAN 20014 0400-P04 INDICATIVE STREET ELEVATIONS 0206-P01 - HOUSETYPE G - ELEVATIONS AND FLOOR PLANS 0205-P01 - HOUSETYPE FF - ELEVATIONS AND FLOOR PLANS 0204-P02 - HOUSETYPE EEEE - ELEVATIONS AND FLOOR PLANS 0203-P01 - HOUSETYPE DD - ELEVATIONS AND FLOOR PLANS 0202-P01 - HOUSETYPE CC - ELEVATIONS AND FLOOR PLANS 0201-P01 - HOUSETYPE AB - ELEVATIONS AND FLOOR PLANS 0200-P02 - HOUSETYPE AA - ELEVATIONS AND FLOOR PLANS</p> <p>P20047-SMCE-ZZ-XXDR-H001 P01 GENERAL LAYOUT P20047-SMCE-ZZ-XX-DR-H-SK01 P01 EASTERN TURNING HEAD TRACKING P20047-SMCE-ZZ-XX-RP-D-0001-P01 VERSION P01 - DRAINAGE STRATEGY REPORT P20047-SMCE-ZZ-XX-DR-D-0001-P01 - DRAINAGE STRATEGY DRAWING</p> <p>COWA-BWB-GEN-01-DR-TR-101 revision P3</p>

	<p>4905/SK06A</p>
--	-------------------

Reason - For the avoidance of doubt and in the interest of proper planning



Created on: 25/06/2020

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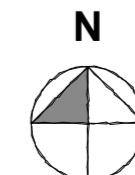
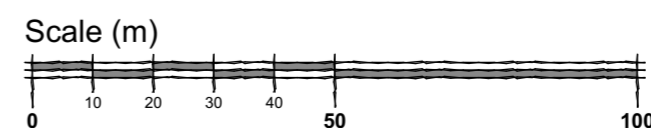
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Rev	Date	Drawn	Note
P02	03/08/2020	JKG	Site boundary line amended.



Rev	Date	Drawn	Note
P02	13/05/2020	JAG	Coordinate amended, Plot 133-137 removed, house type D relocated - following comments from Client.
P03	15/05/2020	JAG	Changes to Plots 21, 25, 130-133 and 136-137 amended.
P04	26/05/2020	JAG	Minor changes relating to highway and drainage scheme, affecting Plots 131-132-135, 136-137 and 139-129.
P05	29/05/2020	JAG	Updated following further input on highway design.
P06	01/06/2020	JAG	Local widening of carriageway and junction (not amended).
P07	03/06/2020	JAG	LEAP added, Plots 5-6 repositioned, site boundary line amended.



- Key**
- Application site boundary
 - Proposed Housing
 - Existing Surrounding Buildings
 - Private Rear Garden Area
 - Public Open / Green Space
 - Principal Highway / Road
 - Private Drive
 - Car Parking Spaces
 - Pedestrian Footpaths
 - Proposed Tree Planting (for illustrative purposes only - subject to detailed design / planting scheme by Landscape Architect)
 - Proposed Hedge / Shrub Planting (for illustrative purposes only - subject to detailed design / planting scheme by Landscape Architect)
 - House types - Primary Point of Access
 - Primary Site Access

schedule of accommodation

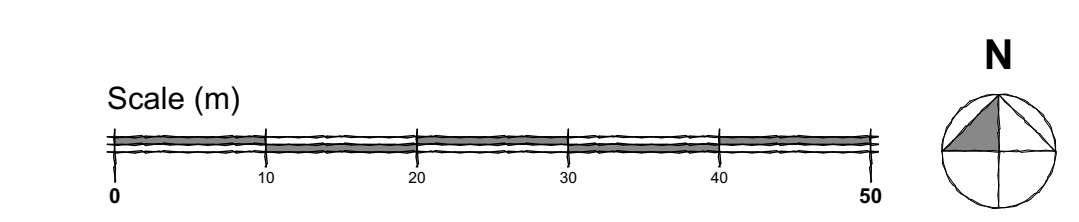
housetype	code	area	quantity
2B4P House	A	72m ²	34 no. units
2B4P House	B	76m ²	14 no. units
3B5P House	C	85m ²	68 no. units
4B6P House	D	105m ²	02 no. units
1B2P Maisonette	E	50m ²	08 no. units
2B3P Bungalow	F	60m ²	06 no. units
3B5P Bungalow	G	78m ²	05 no. units

Total Number of Units : 137 no. units

approximate site area : 12.85 Acres / 5.52Ha

density (based on site area) : 24.8 dwellings per hectare

parking provision : 200% / 100%



Indicative Street Elevations

Scale: 1:200



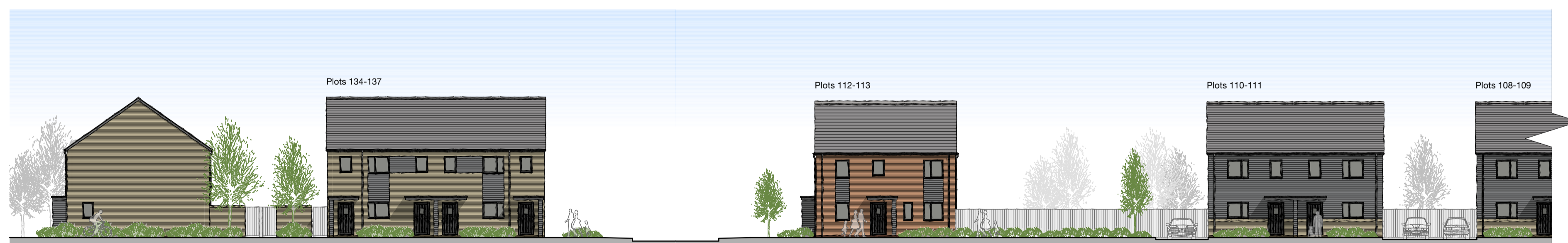
Elevation A~A



Elevation B~B



Elevation C~C



Elevation D~D



Elevation D~D cont...

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Source File:
20014 Model.pln

Rev	Date	Drawn	Note
P02	12/05/2020	JKG	Updated following changes to Site Layout.
P03	15/05/2020	JKG	Elevations D-D updated following changes to site layout.
P04	03/08/2020	JKG	Plots 5-6 repositioned.



Client
Minster Property Group Ltd.

Project Title
Proposed residential development at Land off Elm High Road, Wisbech

Drawing Title
Indicative Street Elevations

Drawing Status
Planning

Scales:
1:200

Drawn:
JKG

Project Number:
20014

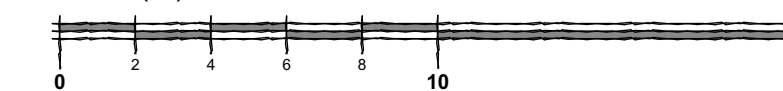
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Original Paper Size:
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Date:
23.04.20

Revision:
P04

Scale (m)



MATERIAL COMBINATION 01



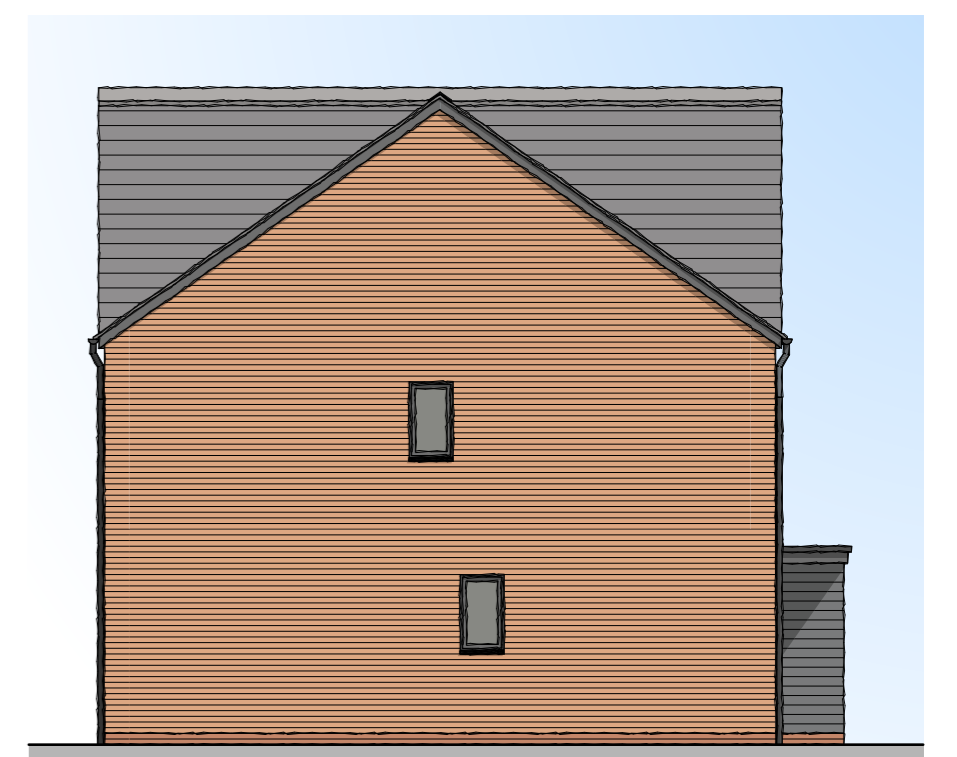
Front Elevation



Side Elevation



Rear Elevation



Side Elevation

MATERIAL COMBINATION 02



Front Elevation



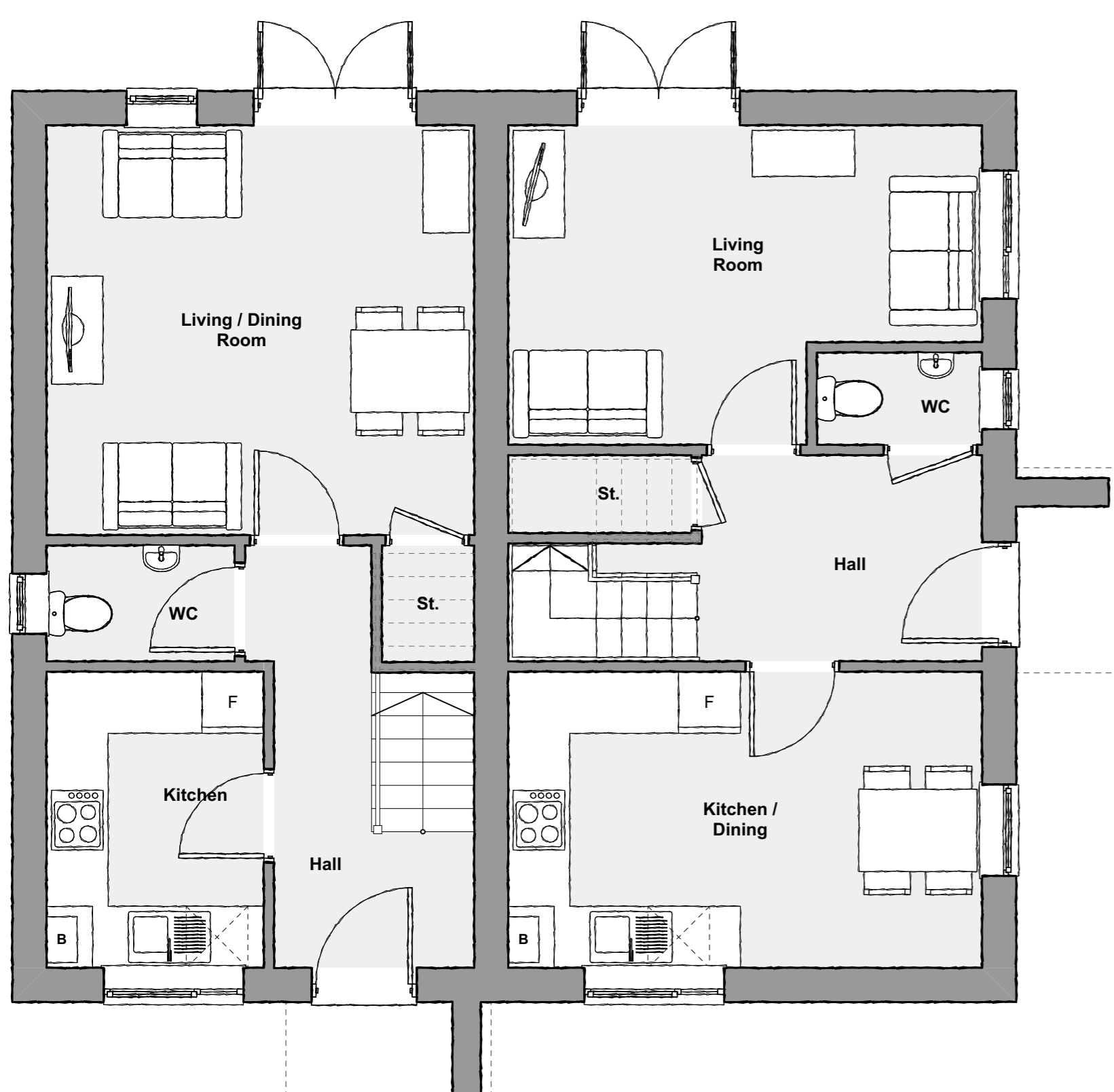
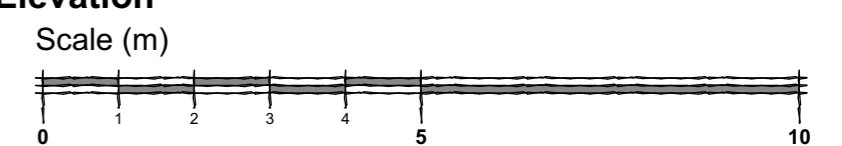
Side Elevation



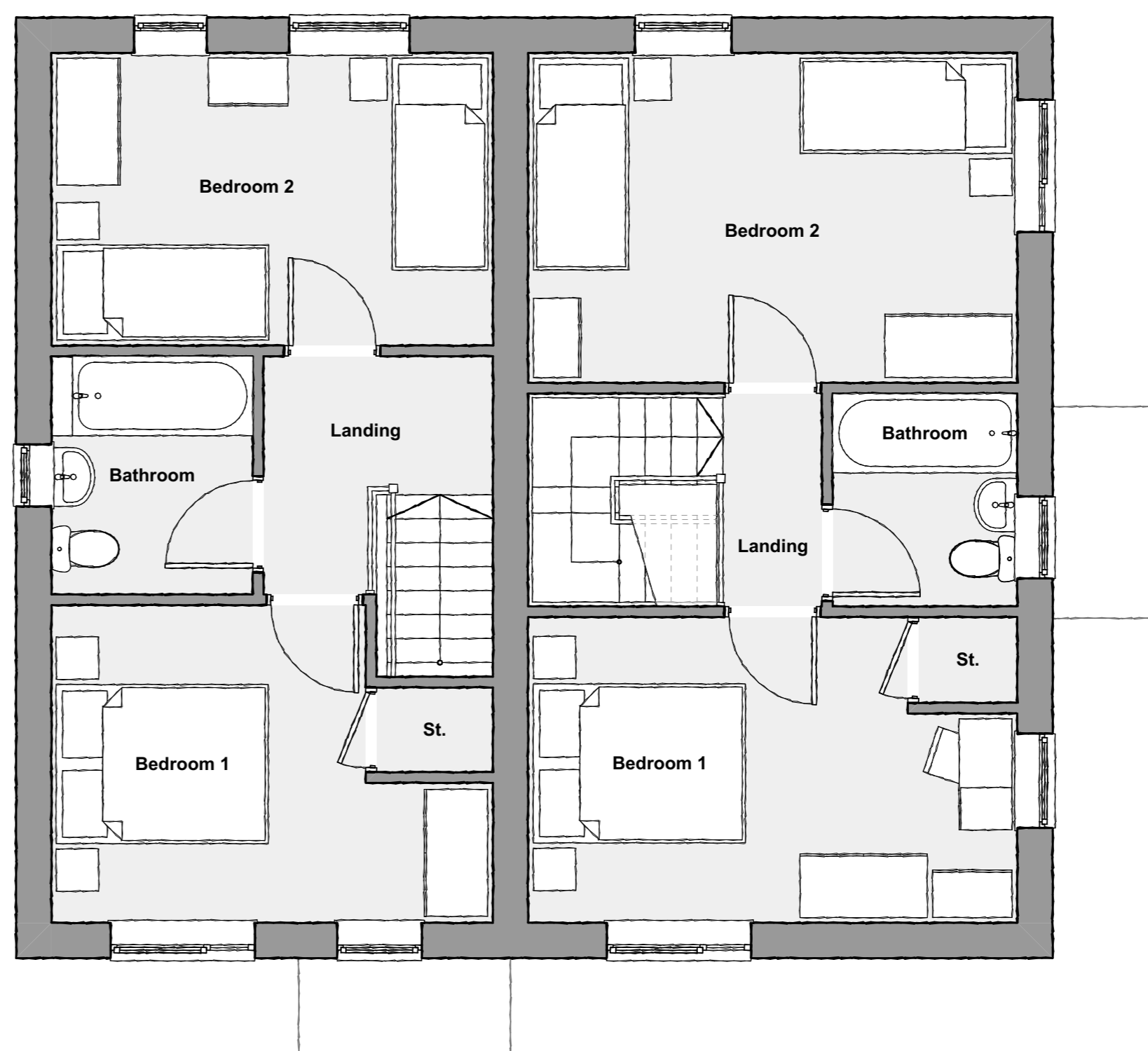
Rear Elevation



Side Elevation



Ground Floor



First Floor

Scale (m)



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Source File:
20014 Model.pln

Rev	Date	Drawn Note



Client:
Minster Property Group Ltd.

Project Title:
Proposed residential development at
Land off Elm High Road,
Wisbech

Drawing Title:
Housetype AB - Plans and
Elevations

Drawing Status:
Planning

Scale(s):
1:50, 1:100

Drawn:
JKG

Project Number:
20014

Drawing Number:
0201

Original Paper Size:
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Date:
22.04.20

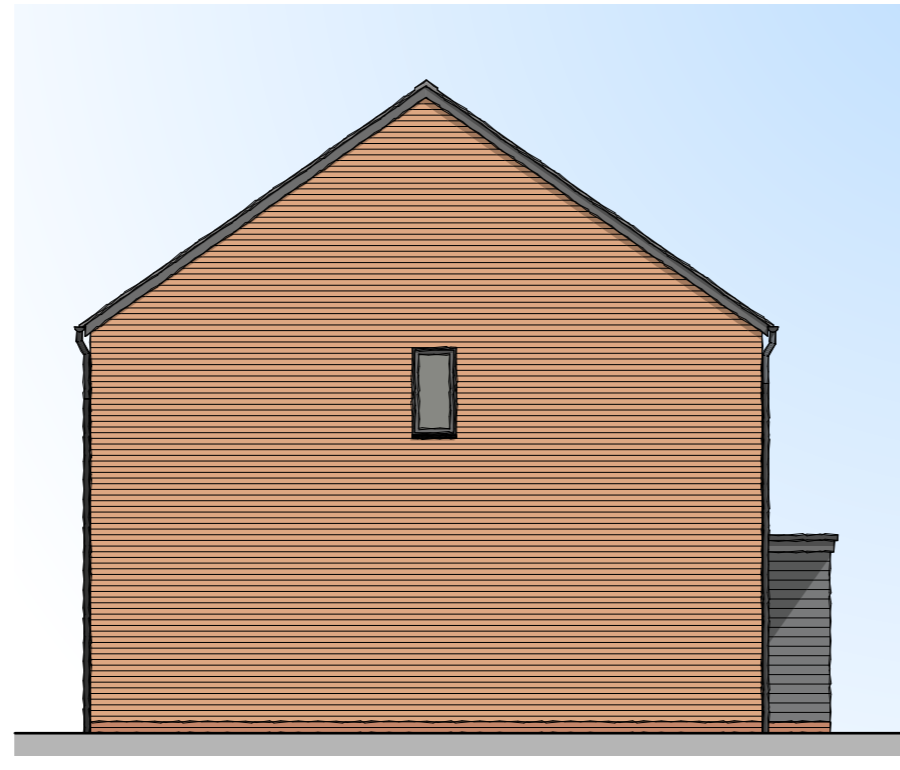
Revision:
P01

Page 53

MATERIAL COMBINATION 01



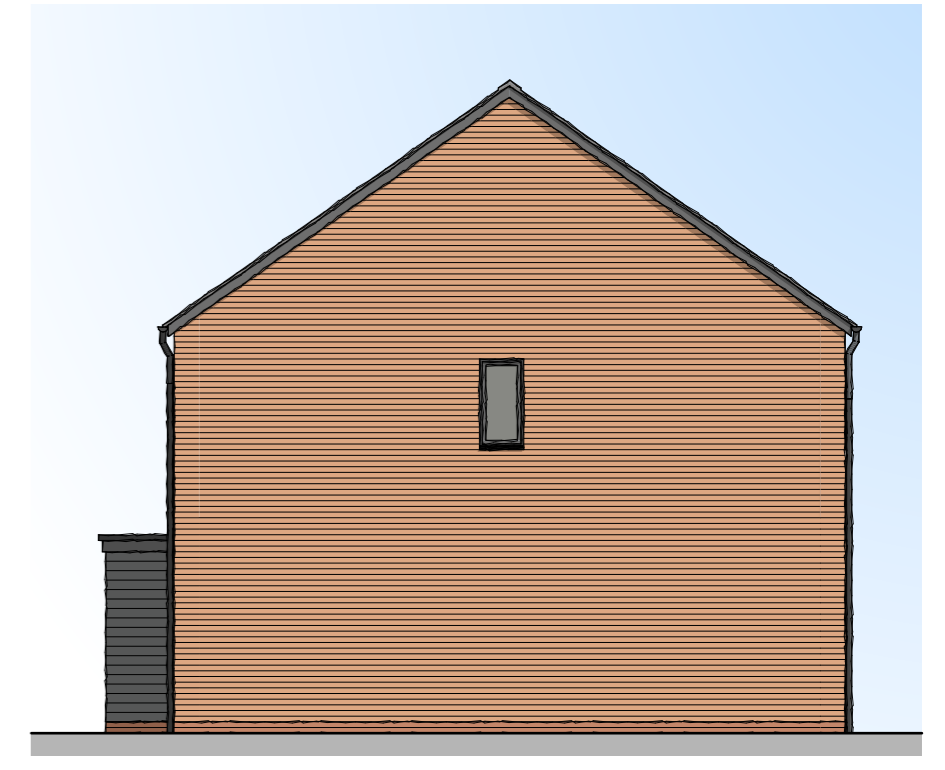
Front Elevation



Side Elevation



Rear Elevation

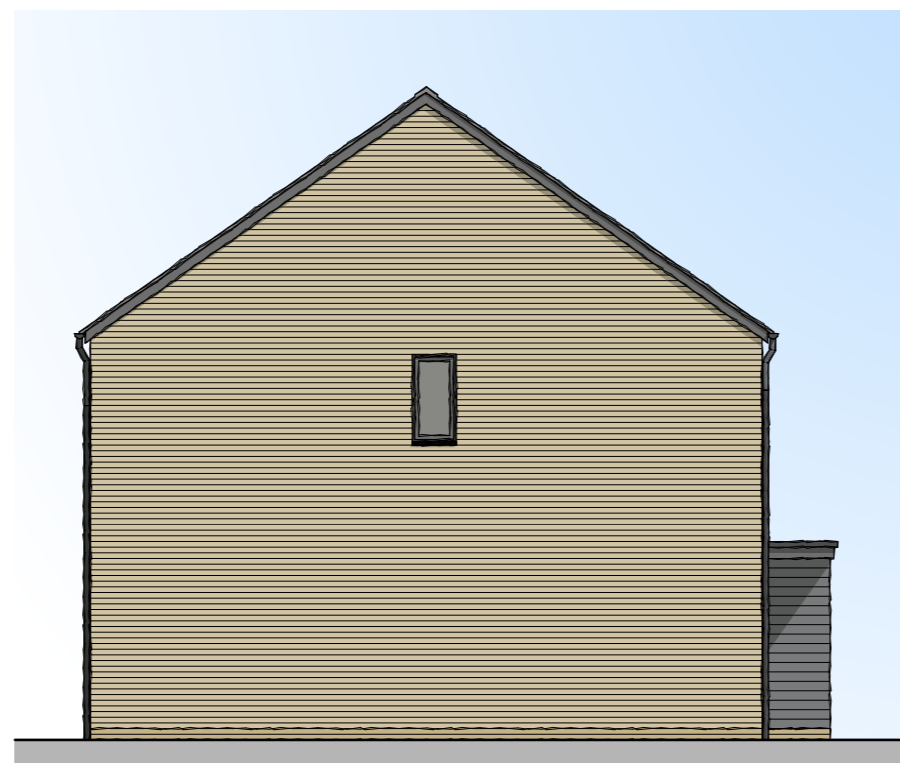


Side Elevation

MATERIAL COMBINATION 02



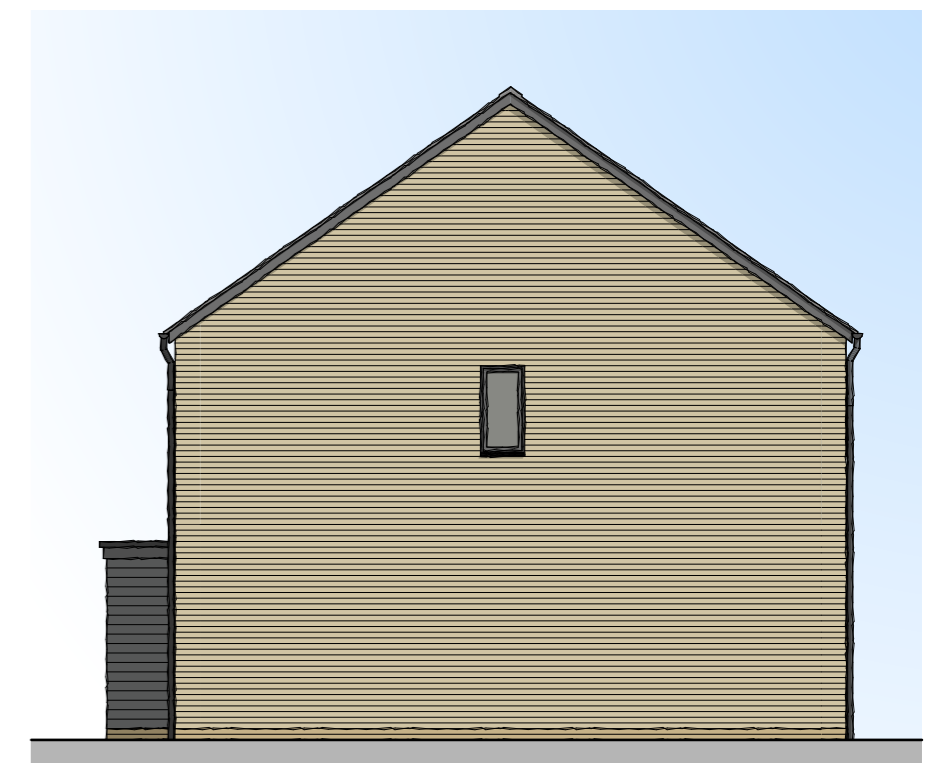
Front Elevation



Side Elevation



Rear Elevation

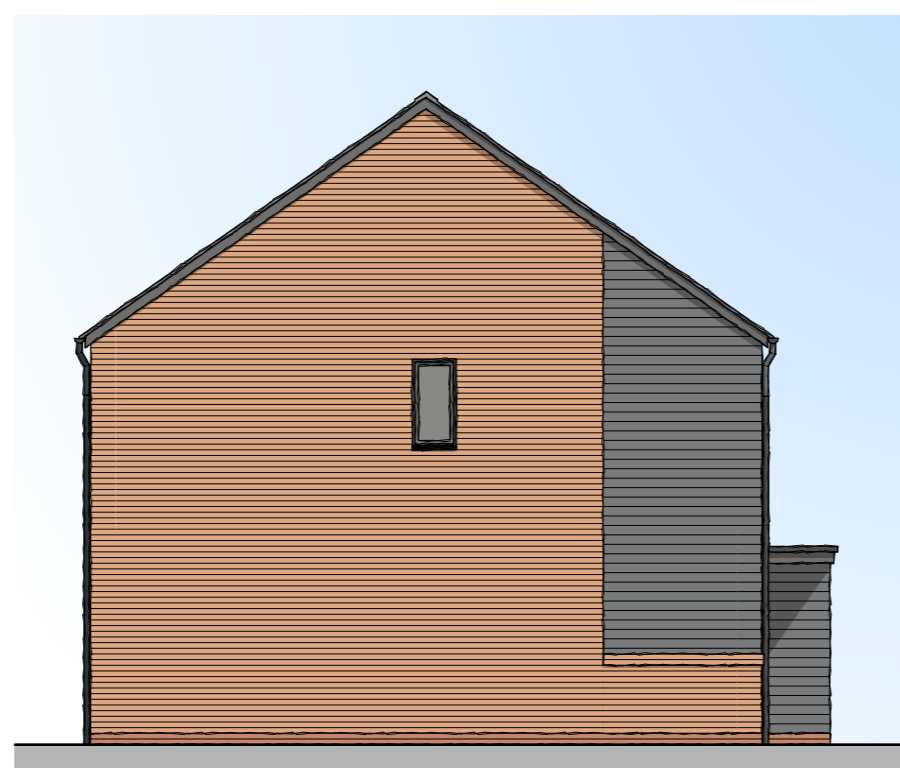


Side Elevation

MATERIAL COMBINATION 03



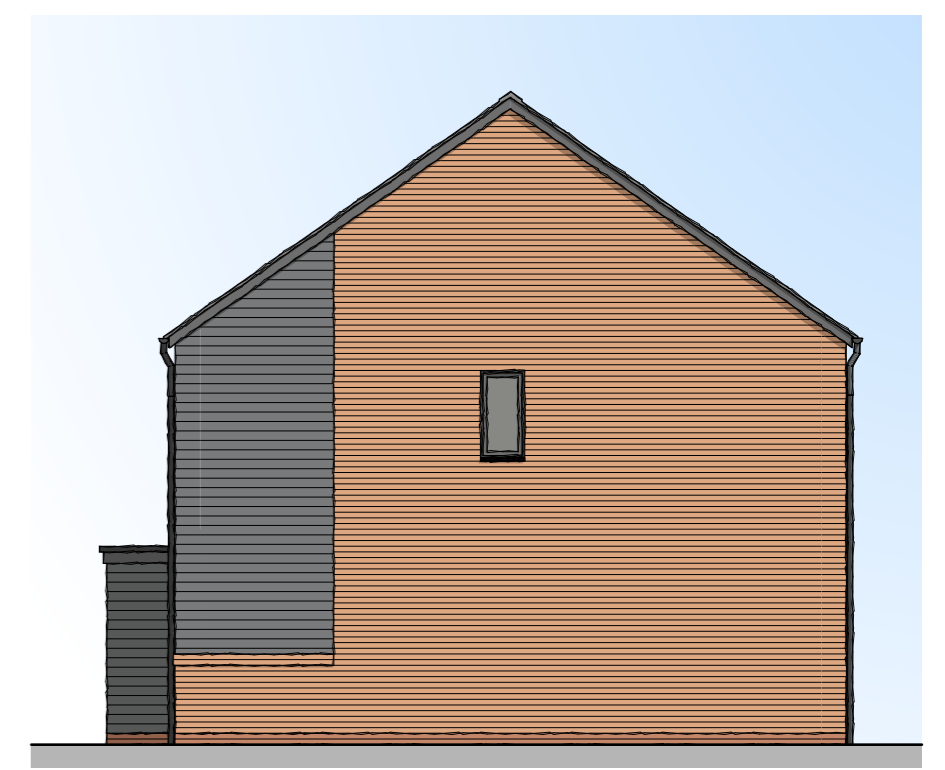
Front Elevation



Side Elevation



Rear Elevation

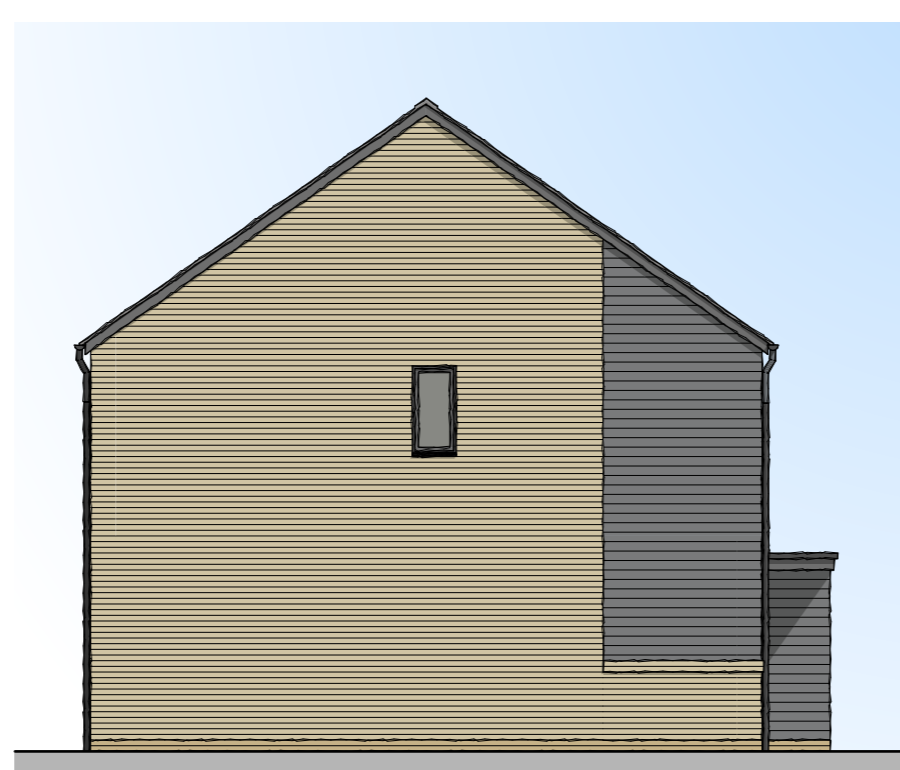


Side Elevation

MATERIAL COMBINATION 04



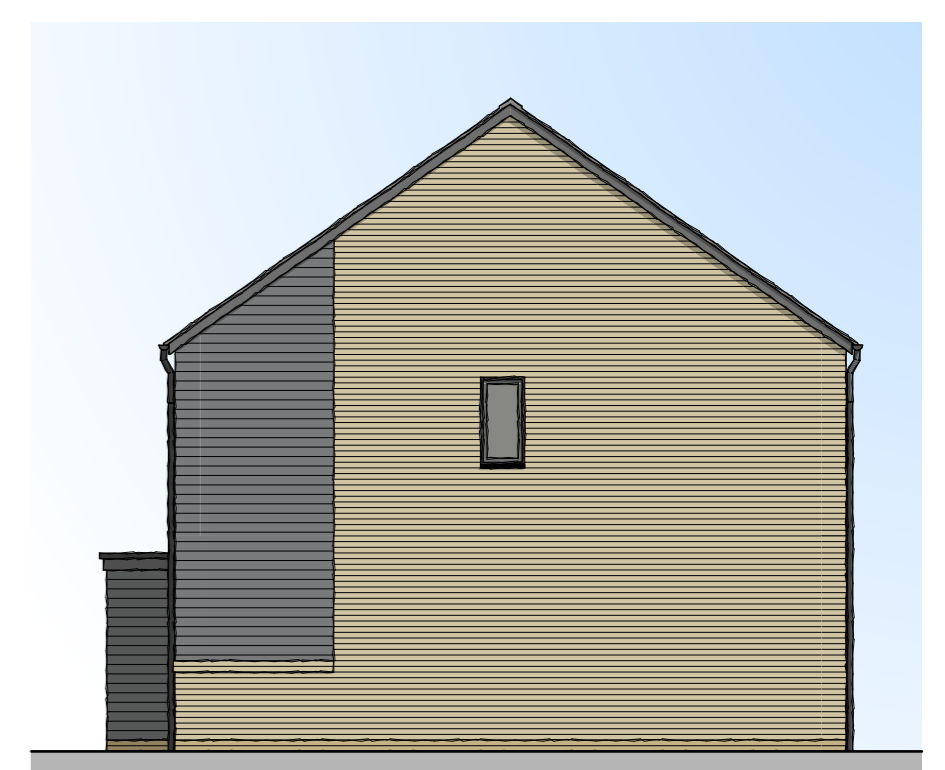
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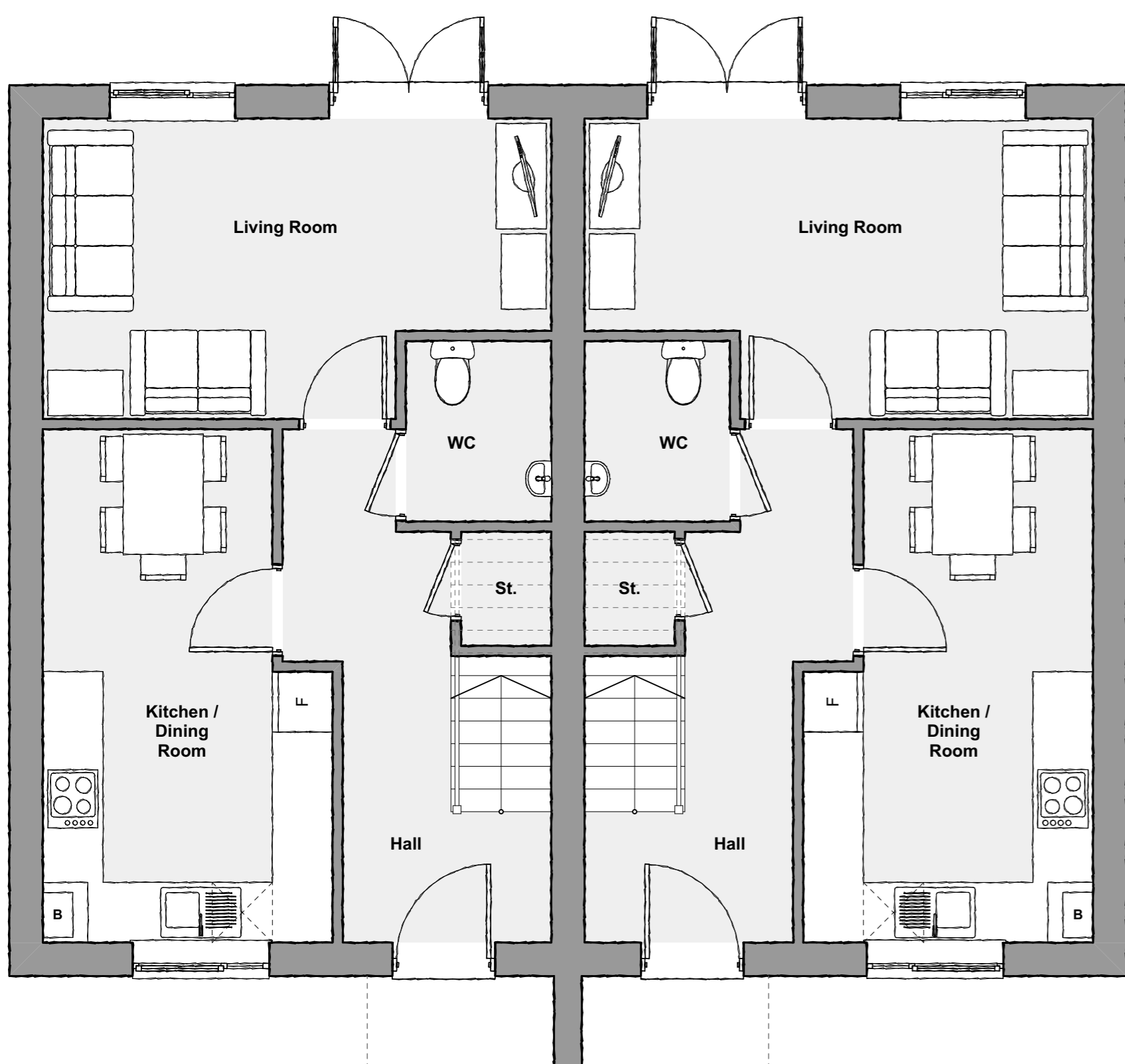
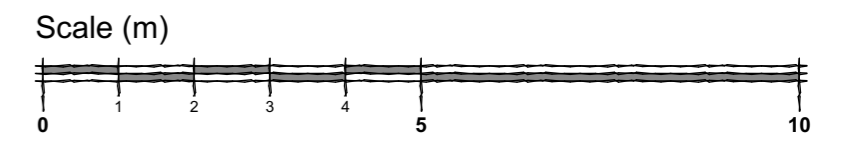
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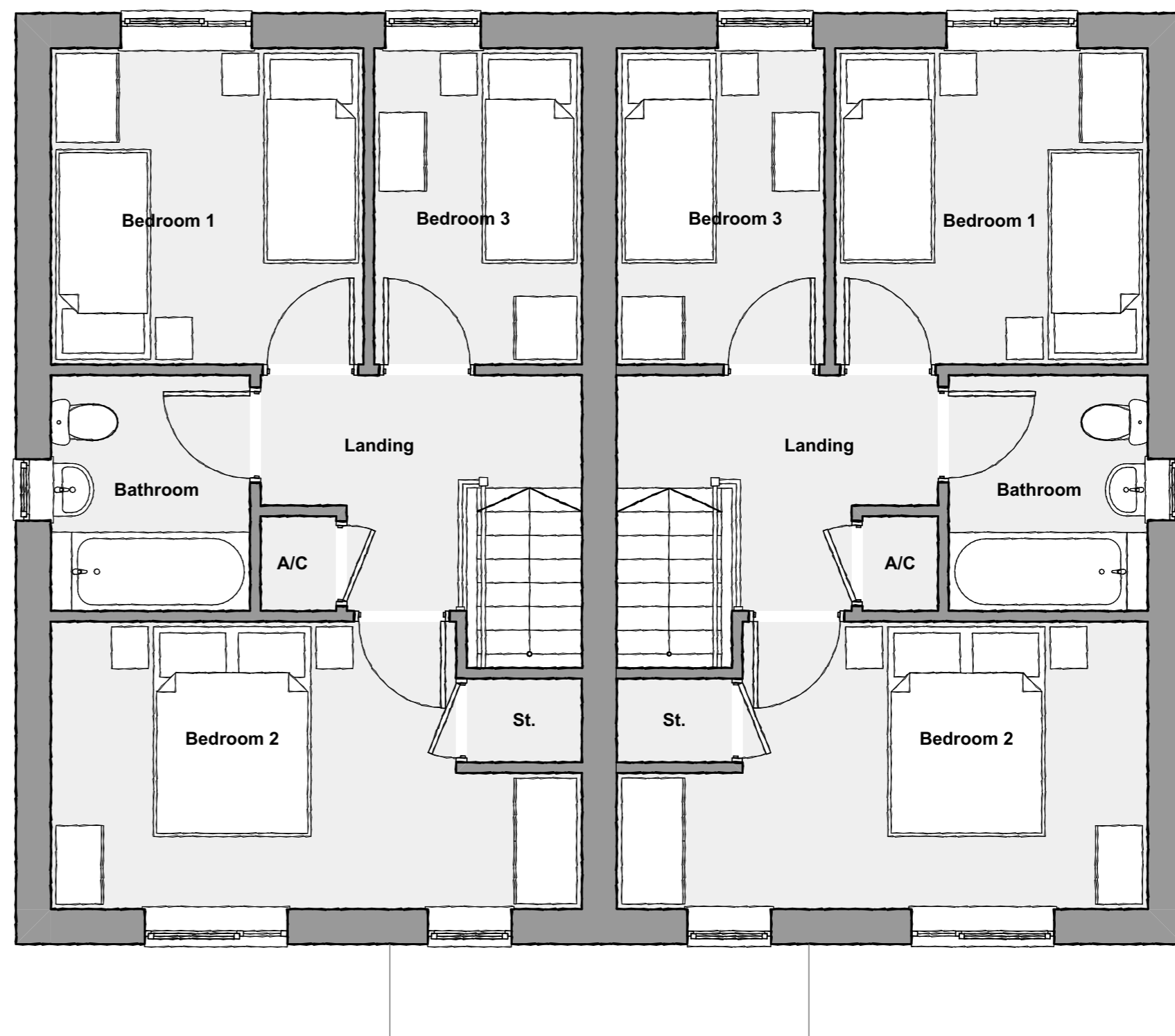
Rear Elevation



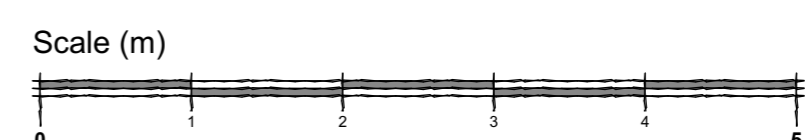
Side Elevation



Ground Floor



First Floor



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Source File: 20014 Model.pln
e: hello@jg-a.co.uk
t: 07429 162 747

Rev	Date	Drawn Note



Client: Minster Property Group Ltd.
Project Title: Proposed residential development at Land off Elm High Road, Wisbech

Drawing Title: Housetype CC - Plans and Elevations
Drawing Status: Planning

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Drawn: JKG	Date: 22.04.20
Project Number: 20014	Drawing Number: 0202
Revision: P01	

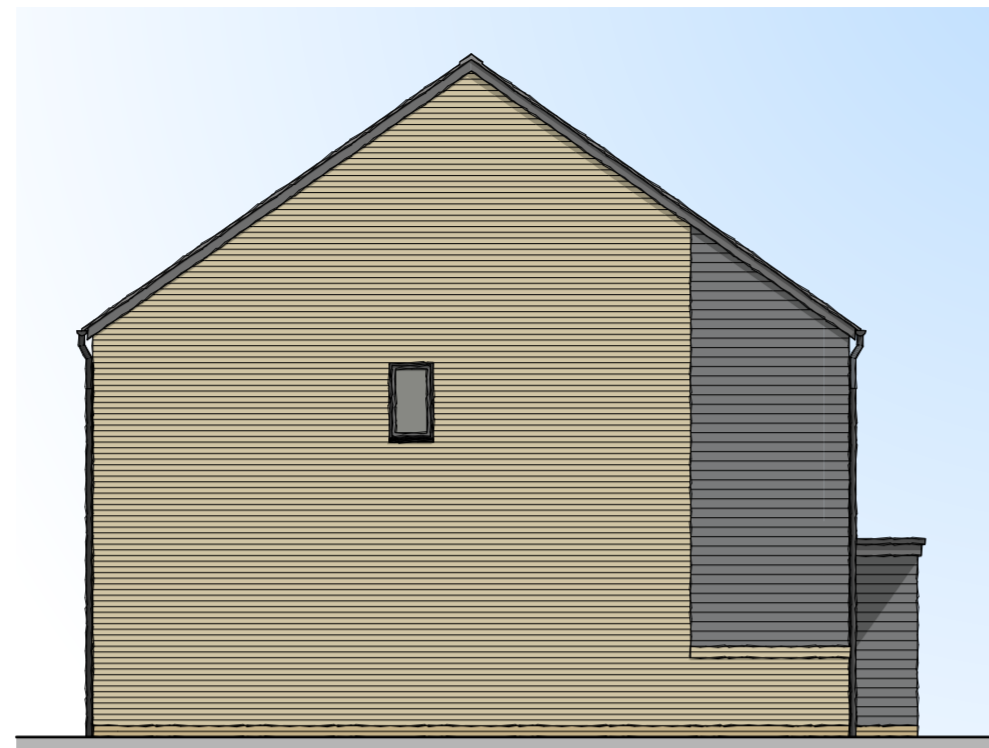
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Scale: 1:50 & 1:100

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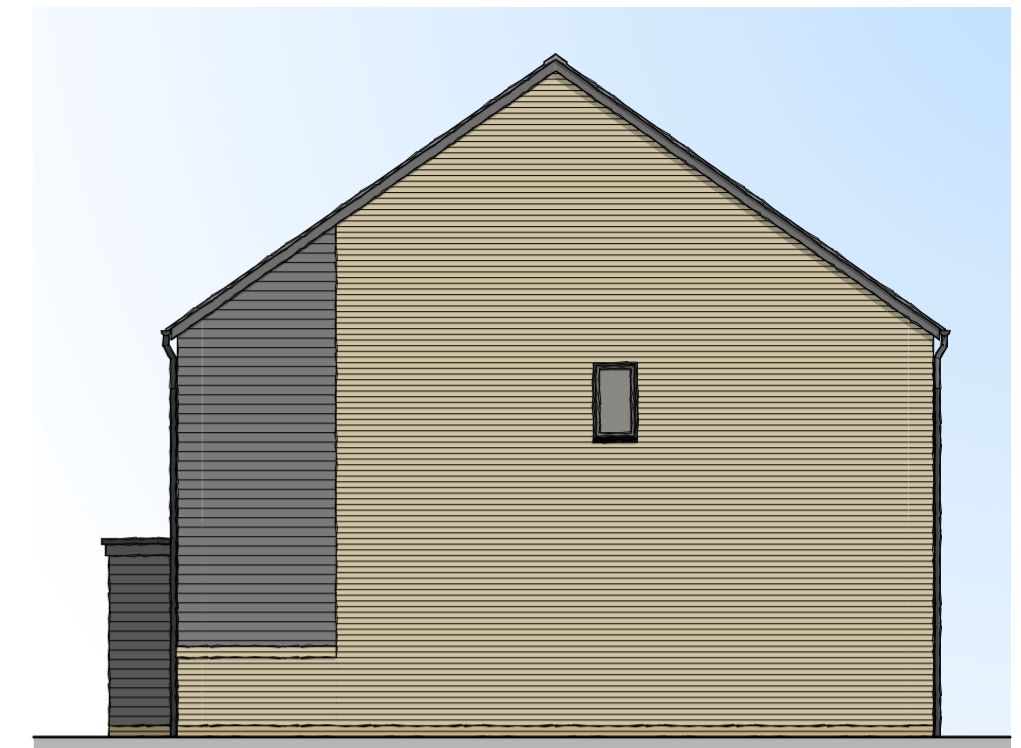
Front Elevation



Side Elevation

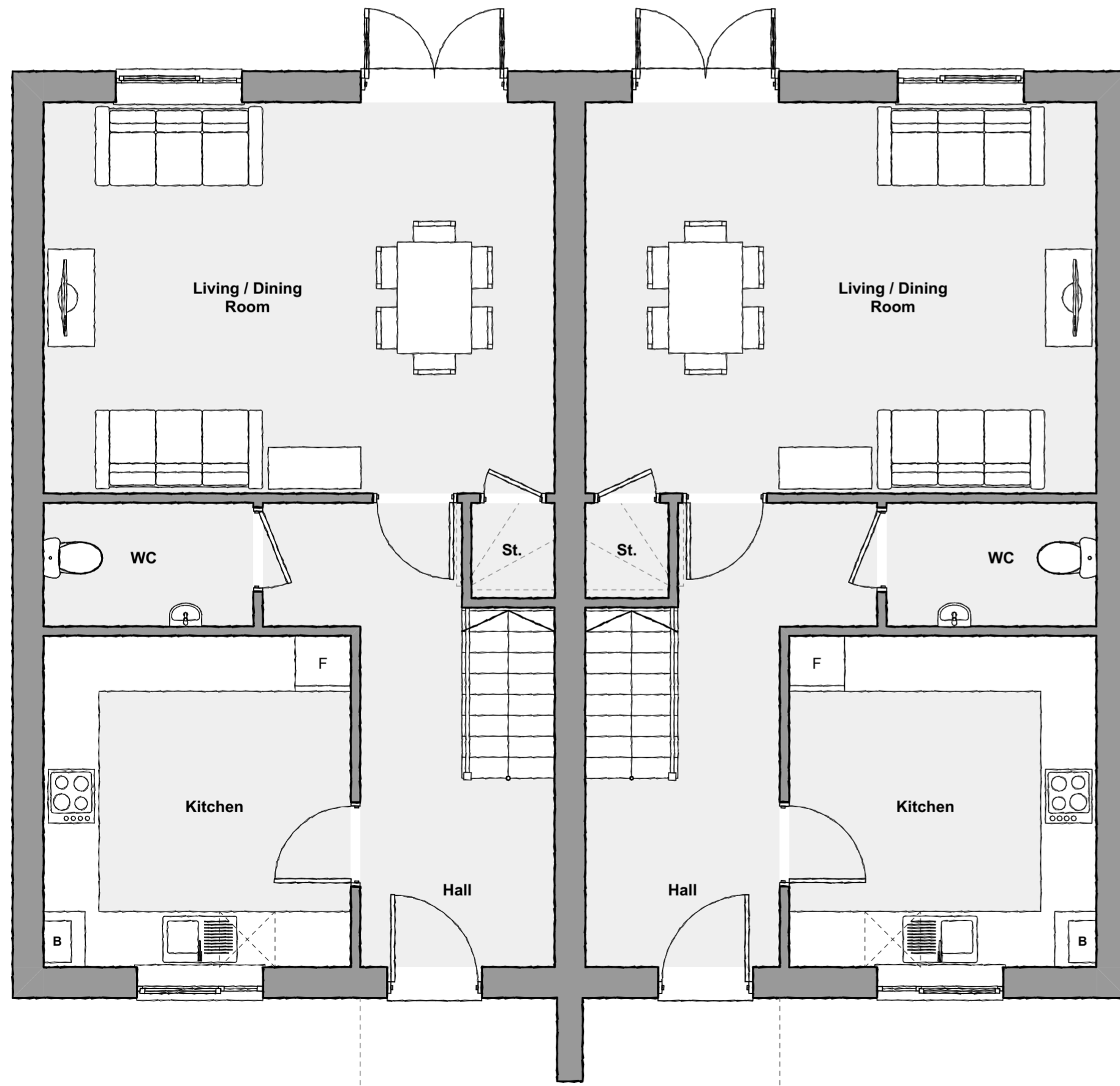
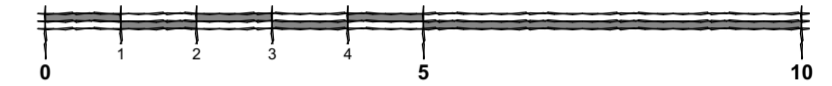


Rear Elevation

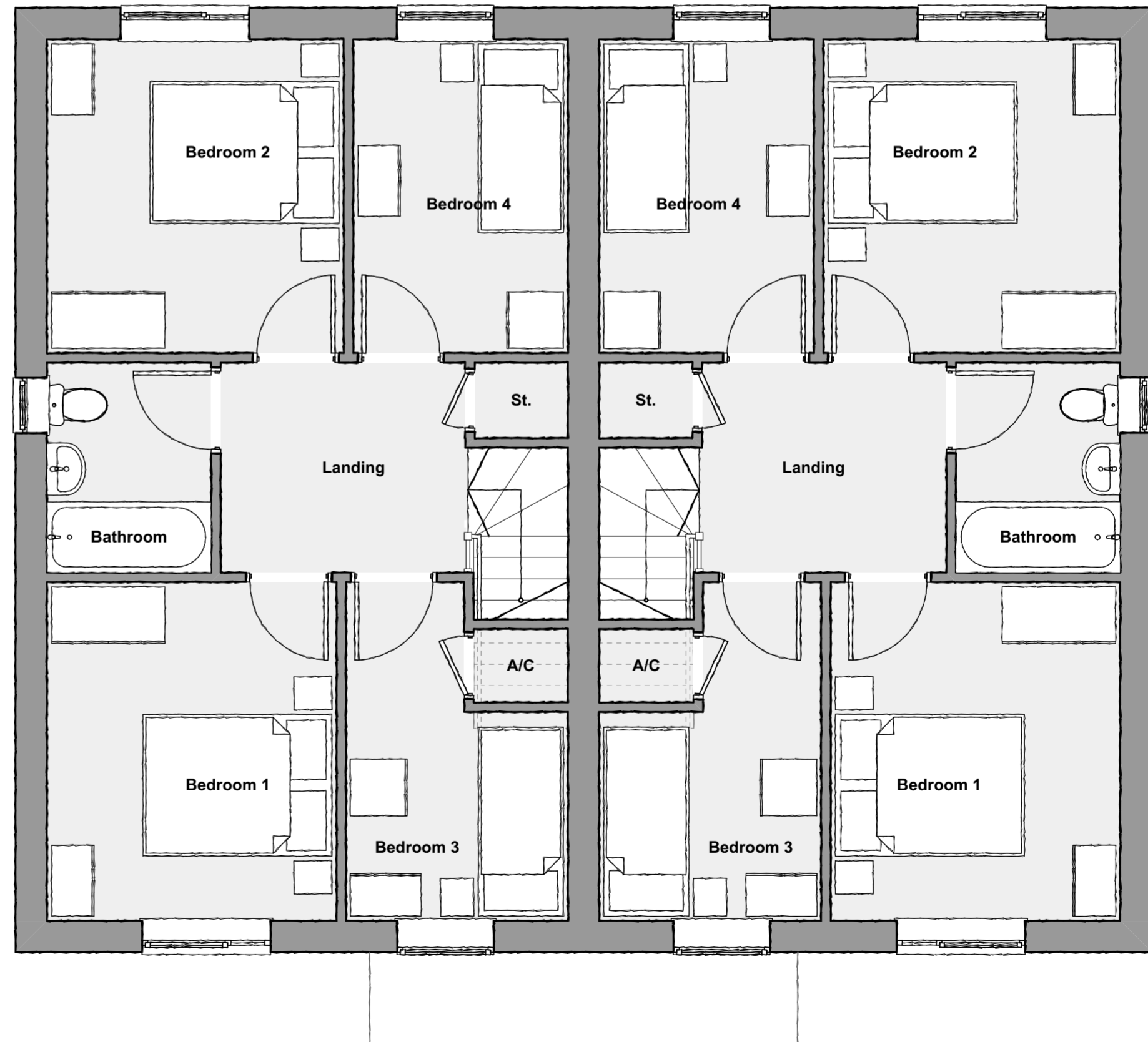


Side Elevation

Scale (m)



Ground Floor



First Floor

Scale (m)



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Source File: 20014 Model.rvt

Rev	Date	Drawn	Note



Client: Minster Property Group Ltd.

Project Title: Proposed residential development at Land off Elm High Road, Wisbech

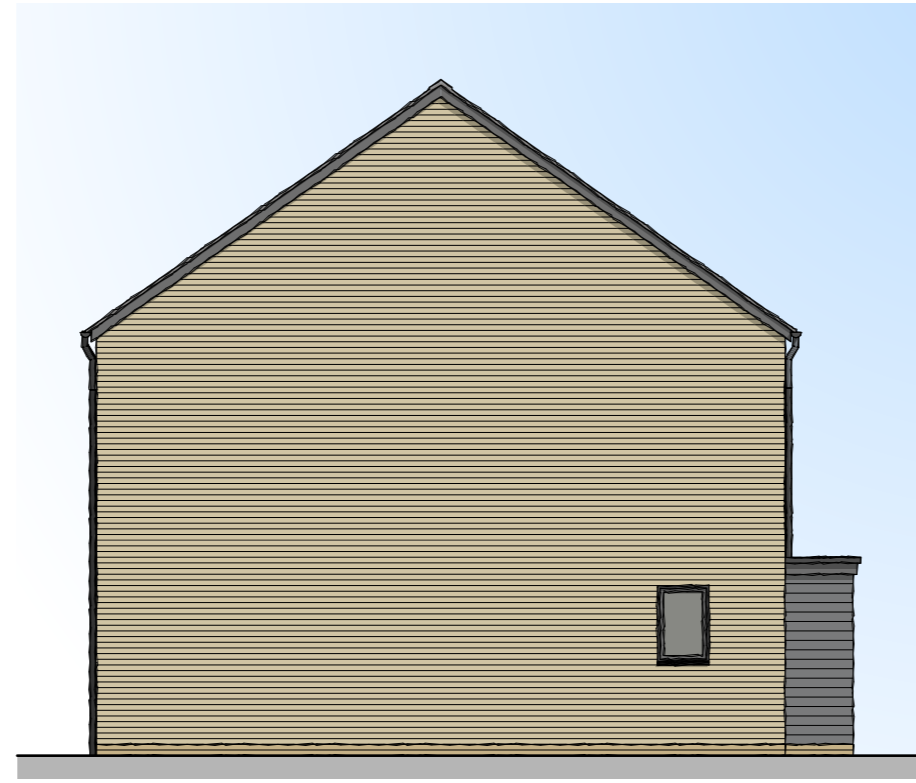
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Drawing Status: Planning

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Drawn: JKG	Date: 22.04.20
Project Number: 20014	Drawing Number: 0203
	Revision: P01



Front Elevation



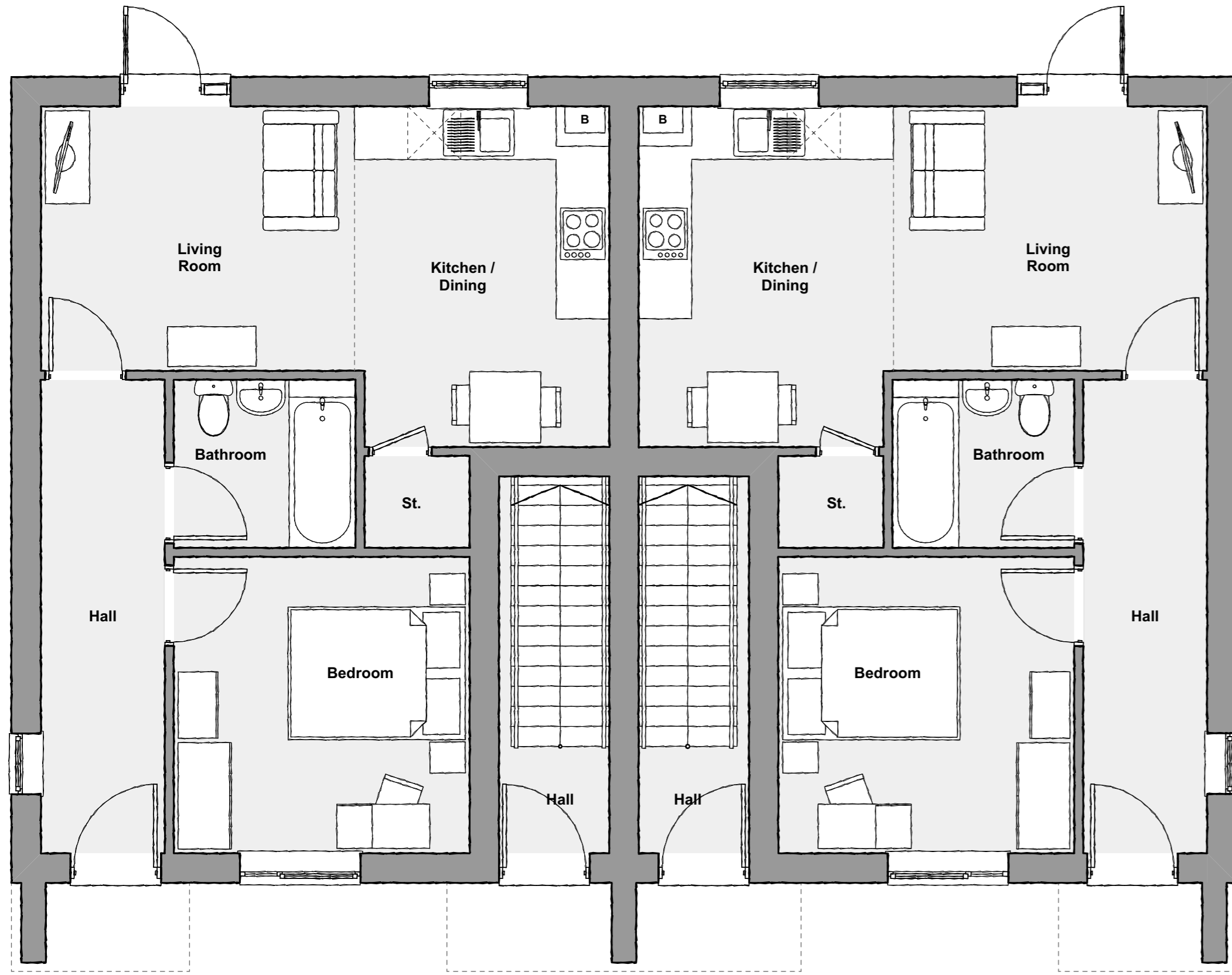
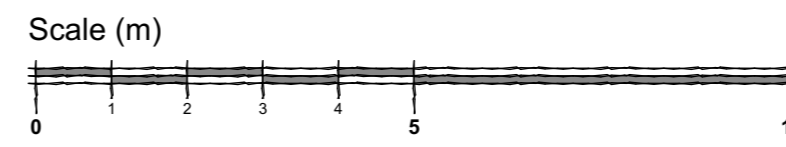
Side Elevation



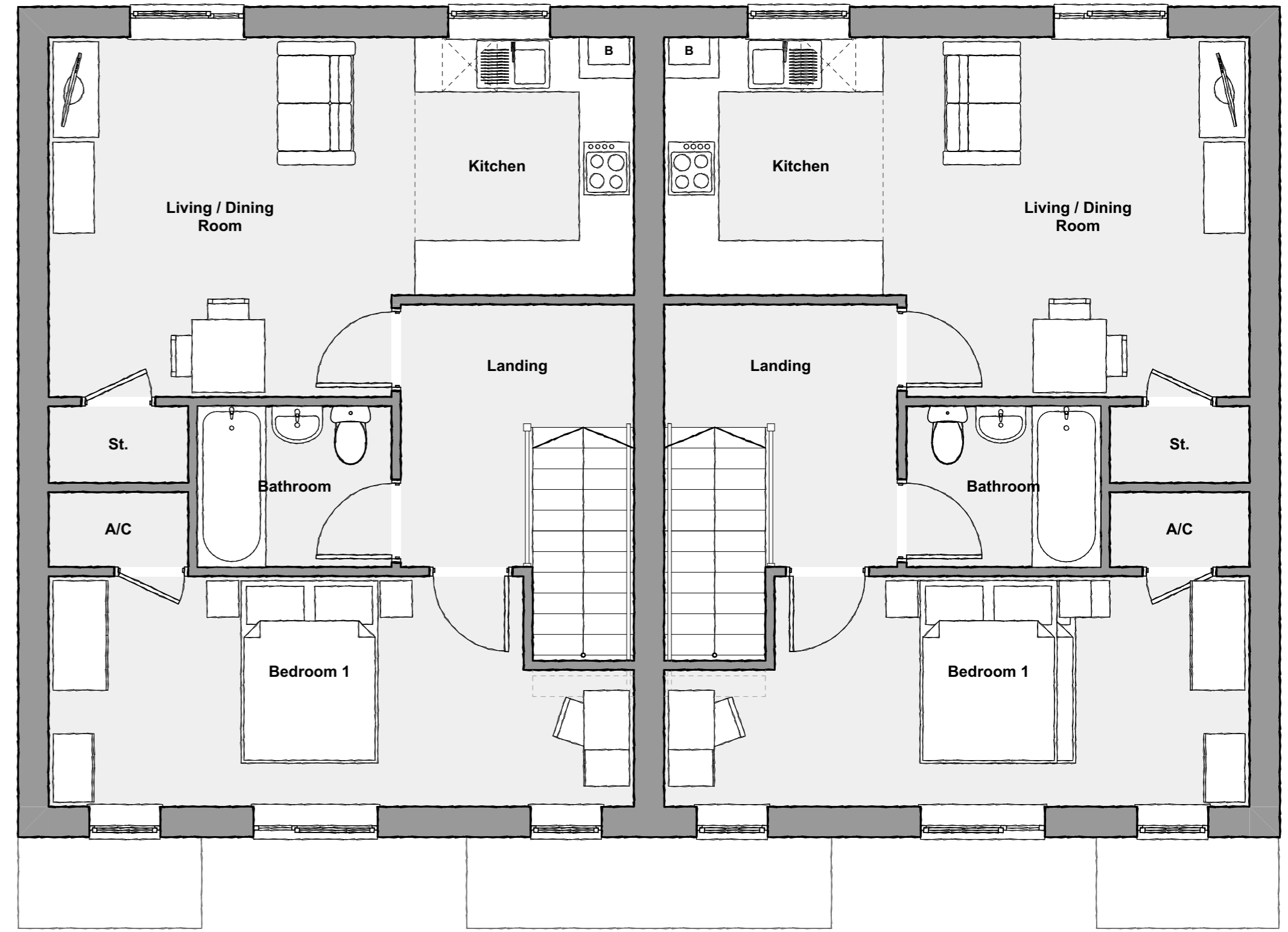
Rear Elevation



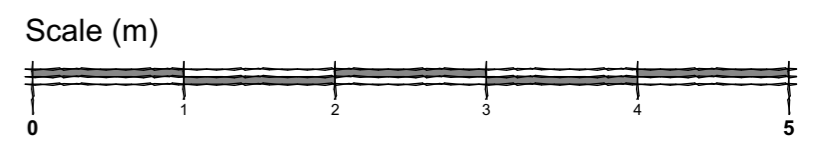
Side Elevation



Ground Floor



First Floor



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Source File:
20014 Model.pln

Rev	Date	Drawn	Note
P02	15/05/2020	JKG	Rear access door to ground floor added.



Client:
Minster Property Group Ltd.

Project Title:
Proposed residential development at Land off Elm High Road, Wisbech

Drawing Title:
Housetype EEEE- Plans and Elevations

Drawing Status:
Planning

Scale(s): 1:50, 1:100	Original Paper Size: A1
Drawn: JKG	Date: 22.04.20
Project Number: 20014	Drawing Number: 0204
	Revision: P02

Housetype FF - Plans & Elevations

Scale: 1:50 & 1:100

MATERIAL COMBINATION 04



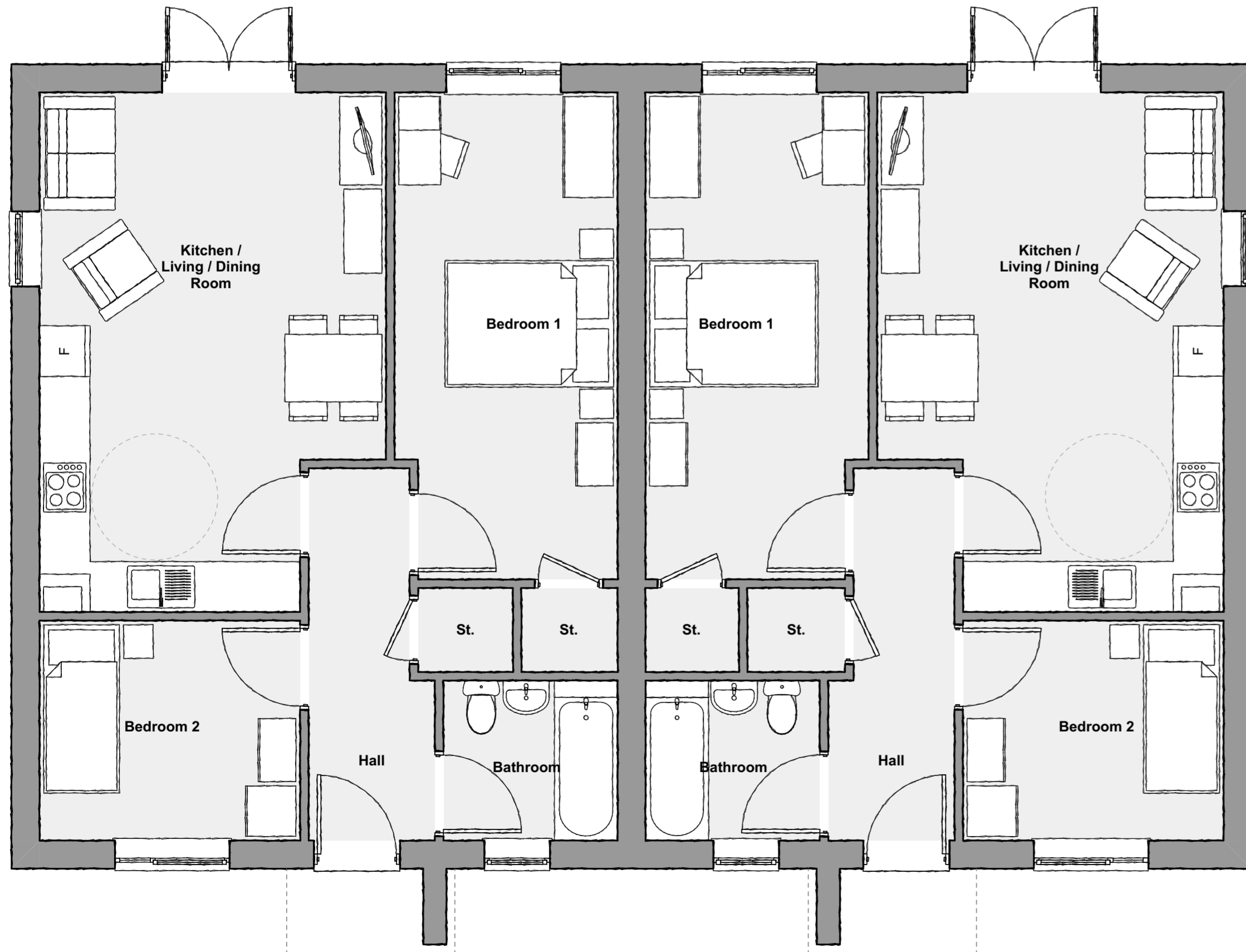
Front Elevation



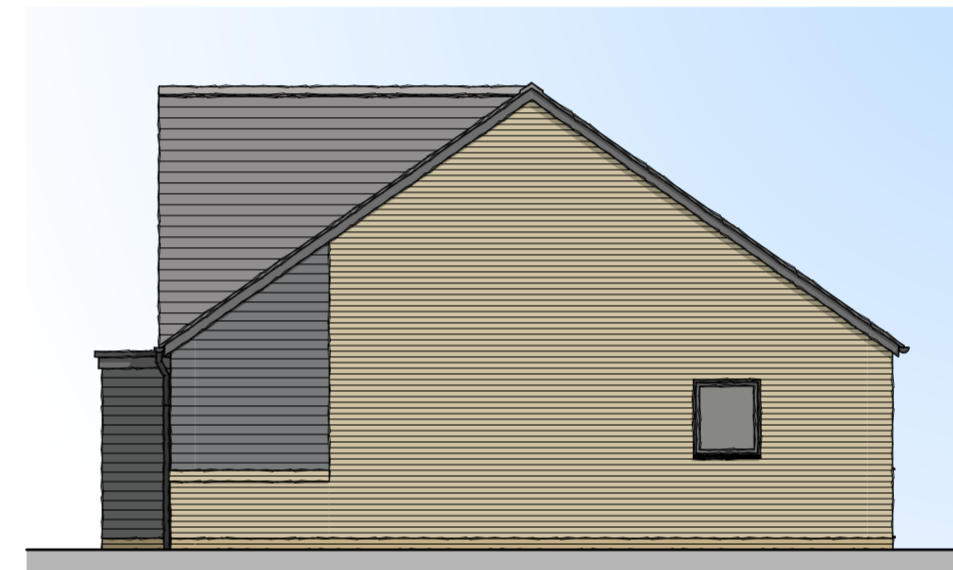
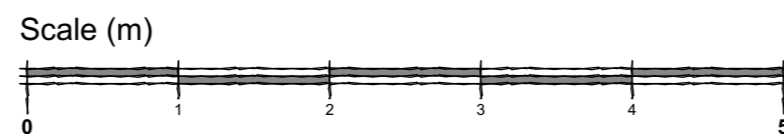
Side Elevation



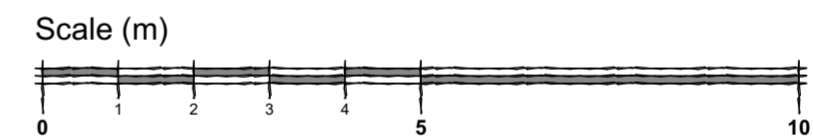
Rear Elevation



Ground Floor



Side Elevation



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Source File: 20014 Model.rvt

Rev	Date	Drawn	Note



MINSTER
PROPERTY GROUP

JGA
ARCHITECTURE & PLANNING
e: hello@jg-a.co.uk t: 07429 162 747

Client
Minster Property Group Ltd.
Project Title
Proposed residential development at
Land off Elm High Road,
Wisbech

Drawing Title
Housetype FF - Plans and
Elevations

Drawing Status
Planning

Scale(s) 1:50, 1:100	Original Paper Size A2
Drawn JKG	Date 22.04.20
Project Number 20014	Drawing Number 0205
	Revision P01

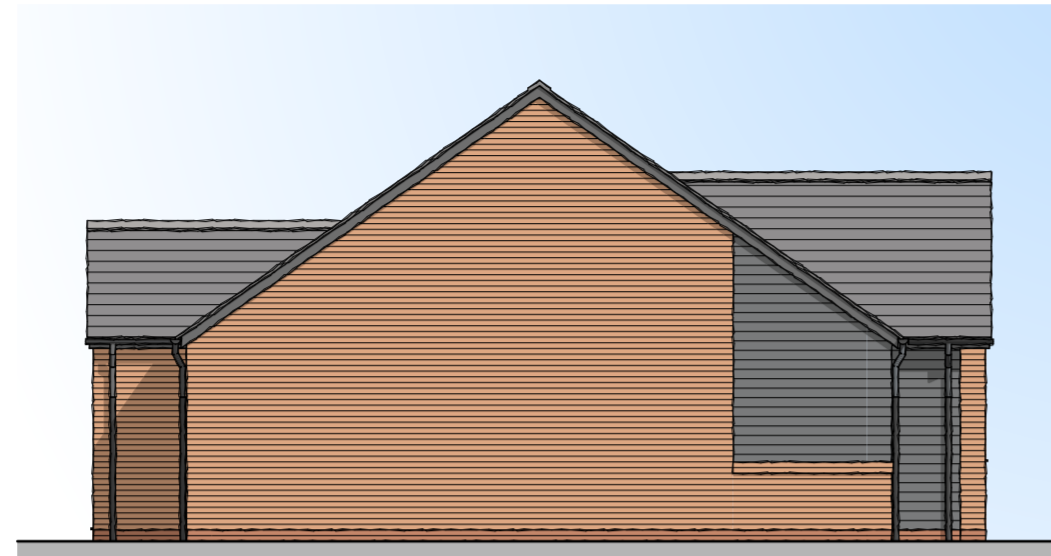
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Scale: 1:50 & 1:100

MATERIAL COMBINATION 03



Front Elevation



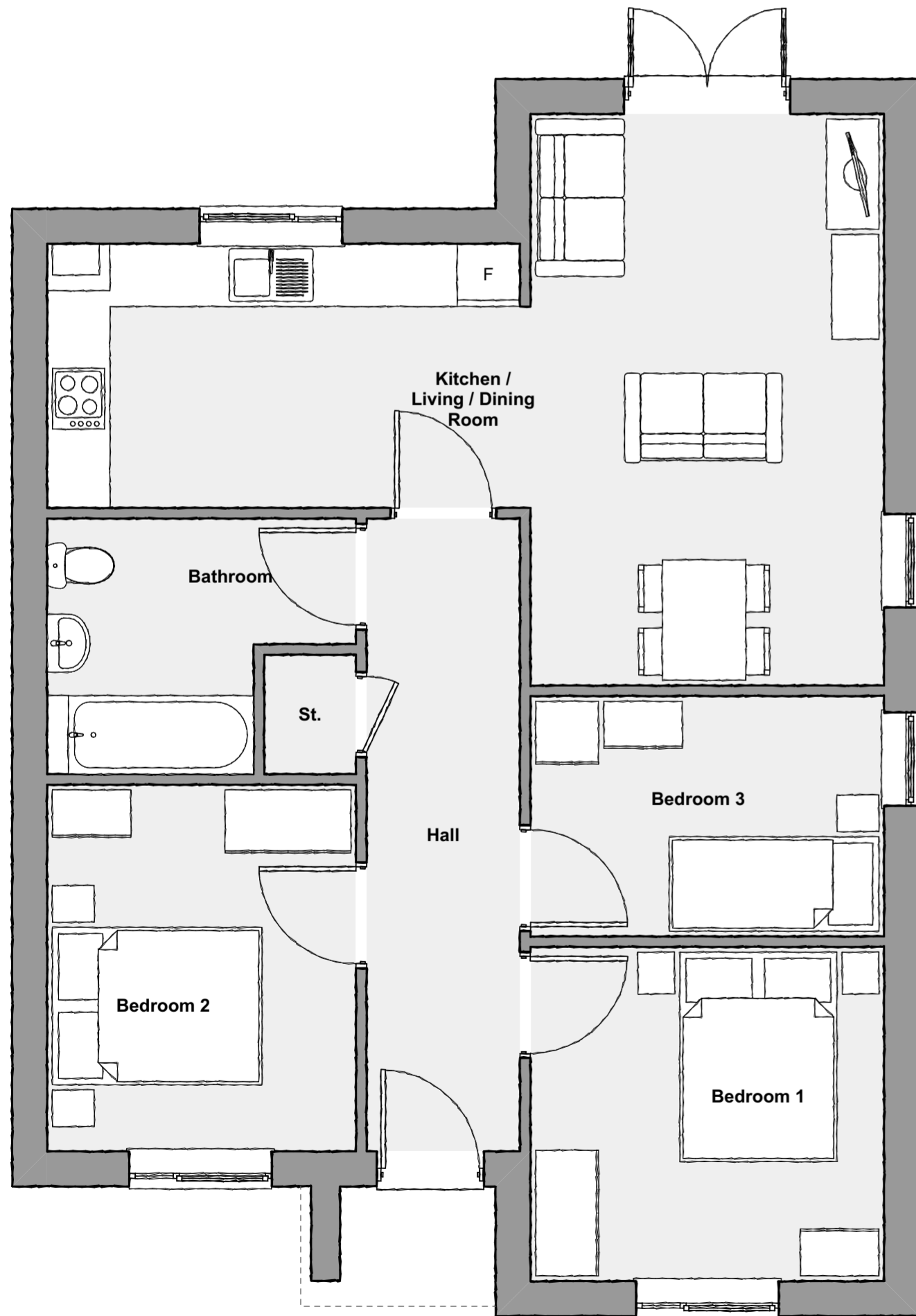
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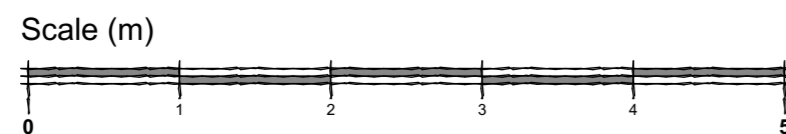
Rear Elevation



Side Elevation



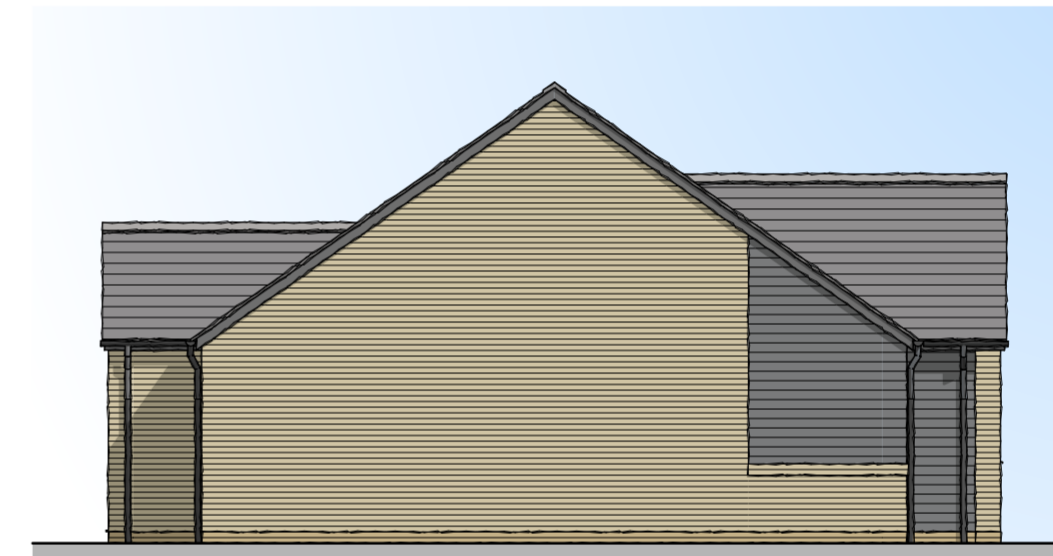
Ground Floor



MATERIAL COMBINATION 04



Front Elevation



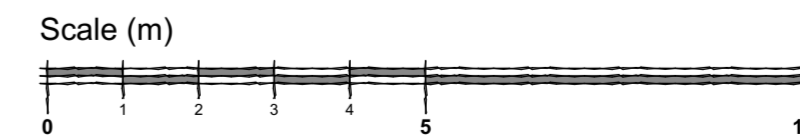
Side Elevation



Rear Elevation



Side Elevation



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Source File: 20014 Model.rvt

Rev	Date	Drawn	Note



Client: Minster Property Group Ltd.

Project Title: Proposed residential development at Land off Elm High Road, Wisbech

Drawing Title: Housetype G - Plans and Elevations

Drawing Status: Planning

Scale(s)	Original Paper Size
1:50, 1:100	A2
Drawn: JKG	Date: 22.04.20
Project Number: 20014	Drawing Number: 0206
	Revision: P01

F/YR20/0576/O

Applicant: Lisa Shepherd

**Agent : Mr Ian Gowler
Gowler Architectural**

Land North West Of 8, Jobs Lane, March, Cambridgeshire

Erect 1 x dwelling (outline application with matters committed in respect of access)

Officer recommendation: Grant

Reason for Committee: Number of representations contrary to Officer recommendation

1 EXECUTIVE SUMMARY

- 1.1 The application seeks outline planning permission of a single dwelling with matters committed in respect of access only.
- 1.2 The application site is located to the rear of 8 Jobs Lane. The principle of tandem development in this area is established, the size of the plot is sufficient to accommodate a dwelling reflective of the area and in excess of a third of the plot is available for private amenity space to serve the host dwelling.
- 1.3 Matters in relation to appearance, landscaping, layout and scale have been reserved for future consideration. However it is considered that only a single-storey dwelling would be acceptable in this location due to the potential impact on visual amenity and the overlooking of, outlook from and overshadowing of adjoining properties. The design of the proposal should be carefully considered to minimise potential impacts on the residential amenity of adjoining dwellings and that of the future occupants of the proposed development.
- 1.4 Access to the site is a matter for consideration. The scheme has been amended during the course of the application to ensure that the required visibility splays are provided, as requested by the Highways Authority. It is considered that suitable parking and turning arrangements can be achieved.
- 1.5 In conclusion, it is considered that subject to detailed design, which requires careful consideration to achieve acceptable levels of visual and residential amenity with onsite parking and turning; the site has the potential to accommodate a policy compliant development. It is therefore recommended to grant the application

2 SITE DESCRIPTION

- 2.1 The application site is garden land serving 8 Jobs Lane. The host property is a detached single-storey dwelling constructed in gold multi brick with a pantile roof. To the front of the site is a garden laid to lawn, enclosed by a low level brick wall and a gravel and concrete parking area. To the rear of the site is a garden area, mainly laid to lawn and a large mobile home is sited.
- 2.2 There is a mix of both 2-storey and single-storey dwellings in the vicinity, of a variety of eras, architectural design and materials. Tandem development is a feature of the area to the north, specifically 10 and 10a Jobs Lane and planning permission has been granted for a dwelling to the rear of 12 Jobs Lane, to the south a new dwelling is under construction to the east of 6 Jobs Lane.

3 PROPOSAL

- 3.1 The application seeks outline planning permission of a single dwelling with matters committed in respect of access only.
- 3.2 Full plans and associated documents for this application can be found at:

<https://www.fenland.gov.uk/publicaccess/applicationDetails.do?activeTab=documents&keyVal=QD1ZHVHE0D800>

4 SITE PLANNING HISTORY

TP10713	Erection of a bungalow	Granted 21/4/1964
OA1929	Erection of a house or bungalow	Granted 18/11/1963

5 CONSULTATIONS

5.1 Environmental Health (FDC)

I have no objections to the proposal, as it is unlikely to have any detrimental effect on air quality or the noise climate. However, I note Photo 1, 2, 3 and 4 of the proposed plans submitted with this application, there are some existing structures and materials on site which may give rise to ground contamination. I would therefore request that the following condition is attached to any planning consent granted:

UNSUSPECTED CONTAMINATION

CONDITION: If during development, contamination not previously identified, is found to be present at the site then no further development (unless otherwise agreed in writing with the LPA) shall be carried out until the developer has submitted, and obtained written approval from the LPA, a Method Statement detailing how this unsuspected contamination shall be dealt with.

REASON: To ensure that the development complies with approved details in the interests of the protection of human health and the environment.

5.2 Environmental Health (FDC) (3/9/2020)

Having reviewed the revised information submitted, I can confirm the Environmental Health team have no further comment to make on this application.

5.3 Town Council (21/7/2020)

Recommend approval.

5.4 Town Council (24/8/2020).

The Town Council have advised their next meeting is not until 7/9/2020 and as such comments are not available at the time of writing the report. Any additional comments will be provided by way of an update.

5.5 Cambridgeshire County Council Highways

The agent's plan makes reference to a visibility splay for a neighbouring development. I am not aware this has been implemented. Until the splay is in use and has been fully implemented, it is not material consideration for the application.

The agent also refers to a splay of 2.5mx48m to the north and 2.5mx40m to the south. Firstly, all that is required for a 30mph road is 2.4mx 43m. Secondly, the agent should avoid splay crossing third party land unless they have the consent of the land owner, in which case the splay should be included within the red edge site curtilage.

I will consider the parking and turning arrangement for the parent property as highway 'gain' in that vehicles for the existing property will be able to enter and leave in a forward gear (post development), noting the reduced visibility to the north.

The agent should detail achievable vehicle to vehicle visibility splay and detail 2mx2m ped' vis' either side of the access. They should also add a note to state the highway access crossover will be amended in accordance with CCC Highway design requirements.

Defer for amended plans

5.6 Cambridgeshire County Council Highways (1/9/2020)

The LHA have no objections subject to the conditions set out at the end of the report.

5.7 Local Residents/Interested Parties

Ten objections have been raised (nine from residents of Church Street and one from a resident of Jobs Lane) in relation to the following:

- Access narrow and on a 'blind bend'
- Visibility
- Historic location
- Will be visible from their property
- Proximity to neighbours
- Plot smaller and narrower than immediate neighbours
- Shared access
- Overcrowding/over development of area
- Detrimental impact on character of the area
- Impact on use of garden
- Noise
- Loss of privacy
- Impact from construction
- Infrastructure already overstretched
- Surface water issues
- Host dwelling has insufficient parking presently and would be made worse

Forty Seven supporting comments have been received (forty two from residents of March, though none in the immediate vicinity, four from residents of Wimblington and one from a resident of Doddington) in relation to the following:

- Need for bungalows and small dwellings
- In keeping with character and appearance
- Proposal would replace mobile home
- No effect on residential amenity
- Host property has a large garden disproportionate to dwelling
- Meets all requirements in respect of parking, boundaries and garden
- Similar developments surrounding
- Improve appearance of plot
- Provides turning/improves parking

Comments, where they relate to planning matters, will be addressed in the sections below.

- 5.8 With regard to the impact of construction, any impact would be of a temporary nature and a development such as this, for a single dwelling, would not warrant a construction management plan.

6 STATUTORY DUTY

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires a planning application to be determined in accordance with the Development Plan unless material planning considerations indicate otherwise. The Development Plan for the purposes of this application comprises the adopted Fenland Local Plan (2014).

7 POLICY FRAMEWORK

National Planning Policy Framework (NPPF)

National Planning Practice Guidance (NPPG)

National Design Guide 2019

Context – C1, C2

Identity – I1, I2

Built Form – B2

Movement – M3

Homes and Buildings – H2, H3

Fenland Local Plan 2014

LP1 – A Presumption in Favour of Sustainable Development

LP2 – Facilitating Health and Wellbeing of Fenland Residents

LP3 – Spatial Strategy, the Settlement Hierarchy and the Countryside

LP4 – Housing

LP5 – Meeting Housing Need

LP9 – March

LP14 – Responding to Climate Change and Managing the Risk of Flooding in Fenland

LP15 – Facilitating the Creation of a More Sustainable Transport Network in Fenland

LP16 – Delivering and Protecting High Quality Environments across the District

LP18 - Historic Environment

Delivering and protecting High Quality Environments in Fenland SPD July 2014

DM3 – Making a Positive Contribution to Local Distinctiveness and Character of the Area

DM4 – Waste and Recycling Facilities

March Neighbourhood Plan 2017

H2 – Windfall Development

H3 – Local Housing Need

8 KEY ISSUES

- **Principle of Development**
- **Heritage**
- **Design considerations, visual and residential amenity**
- **Highways/parking**
- **Flood Risk**

9 ASSESSMENT

Principle of Development

- 9.1 The application site is located within the settlement of March which is identified within the Settlement Hierarchy as a Primary Market Town; Market Towns are identified within Policy LP3 as the focus for housing growth, accordingly there is a presumption in favour of development within this location. This is however on the basis that the development is in keeping with and reflects the character of the area and that there are no significant issues in respect of heritage, residential or visual amenity, design, parking, highways and flood risk.

Heritage

- 9.2 The application site is located in the vicinity of St Wendreda's Church, a Grade I listed building, in the historic core of March. An application for the site adjoining (F/YR17/0977/F) required an archaeological investigation to be undertaken, for this reason Cambridgeshire County Council Archaeology have been contacted for advice on whether that would also be the case for this site. Their comments will be provided by way on an update.
- 9.3 The site is surrounded by modern development and as such is not considered to have a detrimental impact on the setting of St Wendreda's Church.

Design considerations, visual and residential amenity

- 9.4 The application site is located to the rear of 8 Jobs Lane, there is tandem development to the north, specifically 10 and 10a Jobs Lane and permission was granted in March and June 2019 for a dwelling to the rear of 12 Jobs Lane (F/YR19/0291/F and F/YR19/0581/F). To the south of the site a dwelling is under construction to the east of 6 Jobs Lane, though it is acknowledged both of these properties have a street presence. There is a mix of both 2-storey and single-storey dwellings in the vicinity, of a variety of eras, architectural design and materials.
- 9.5 The principle of tandem development in this area is established, the size of the plot is sufficient to accommodate a dwelling reflective of the area and in excess of a third of the plot is available for private amenity space to serve the host dwelling.
- 9.6 Matters in relation to appearance, landscaping, layout and scale have been reserved for future consideration. However it is considered that only a single-storey dwelling would be acceptable in this location due to the potential impact on visual amenity and the overlooking of, outlook from and overshadowing of adjoining properties.

- 9.7 There are 2-storey dwellings to the rear of the site which result in the site being overlooked and dwellings to the north and south are located relatively close to the boundaries, hence the siting and design should be carefully considered. However, there is scope for any proposal to be designed in such a way to minimise the harm created in respect overlooking, overshadowing and loss of light and outlook, achieve acceptable distances and the required level of private amenity space.
- 9.8 It is acknowledged that it is likely that development of this site would result in bins being carried in excess of the recommended 30m and would therefore be contrary to RECAP guidance and Policy DM4 of the Delivering and protecting High Quality Environments in Fenland SPD 2014. This is however not considered to be sufficiently detrimental to warrant a refusal in this regard and the provision of a bound surface to the access (to be conditioned) would allow for ease of transfer from storage to collection areas.
- 9.9 It is considered that an appropriate design solution in accordance with Policy LP2 and LP16 of the Fenland Local Plan, DM3 of the Delivering and protecting High Quality Environments in Fenland SPD 2014, Chapter 12 of the NPPF 2019 and Chapters C1, I1, I2, B2 and H1 of the NDG 2019 can be achieved.

Highways/parking

- 9.10 Access to the site is a matter for consideration and has been amended during the course of the application to ensure that the required visibility splays, as requested by the Highways Authority can be achieved.
- 9.11 Layout and therefore the internal parking and turning arrangements have not been committed, however the application site incorporates land which enables parking to the front of the site for the host dwelling and for the site to be exited in forward gear, which is an improvement on the present situation. It is considered that suitable arrangements can be achieved in accordance with Policy LP15 of the Fenland Local Plan 2014 and M3 of the NDG 2019.

Flood Risk

- 9.12 The application site falls within Flood Zone 1 (low risk) and as such the proposal is considered to be appropriate development and does not require the submission of a flood risk assessment or inclusion of mitigation measures. Issues of surface water will be considered under Building Regulations; accordingly there are no issues to address in respect of Policy LP14.

10 CONCLUSIONS

- 10.1 This report has considered the issues relevant to the proposal and concluded that the development would be consistent with Policies LP1, LP2, LP3, LP4, LP5, LP9, LP14, LP15, LP16 and LP18 of the Fenland Local Plan and the aims of the NPPF and NDG 2019. It is considered that subject to detailed design, which requires careful consideration to achieve acceptable levels of visual and residential amenity with onsite parking and turning; the site has the potential to accommodate a policy compliant development and a favourable recommendation is therefore forthcoming.

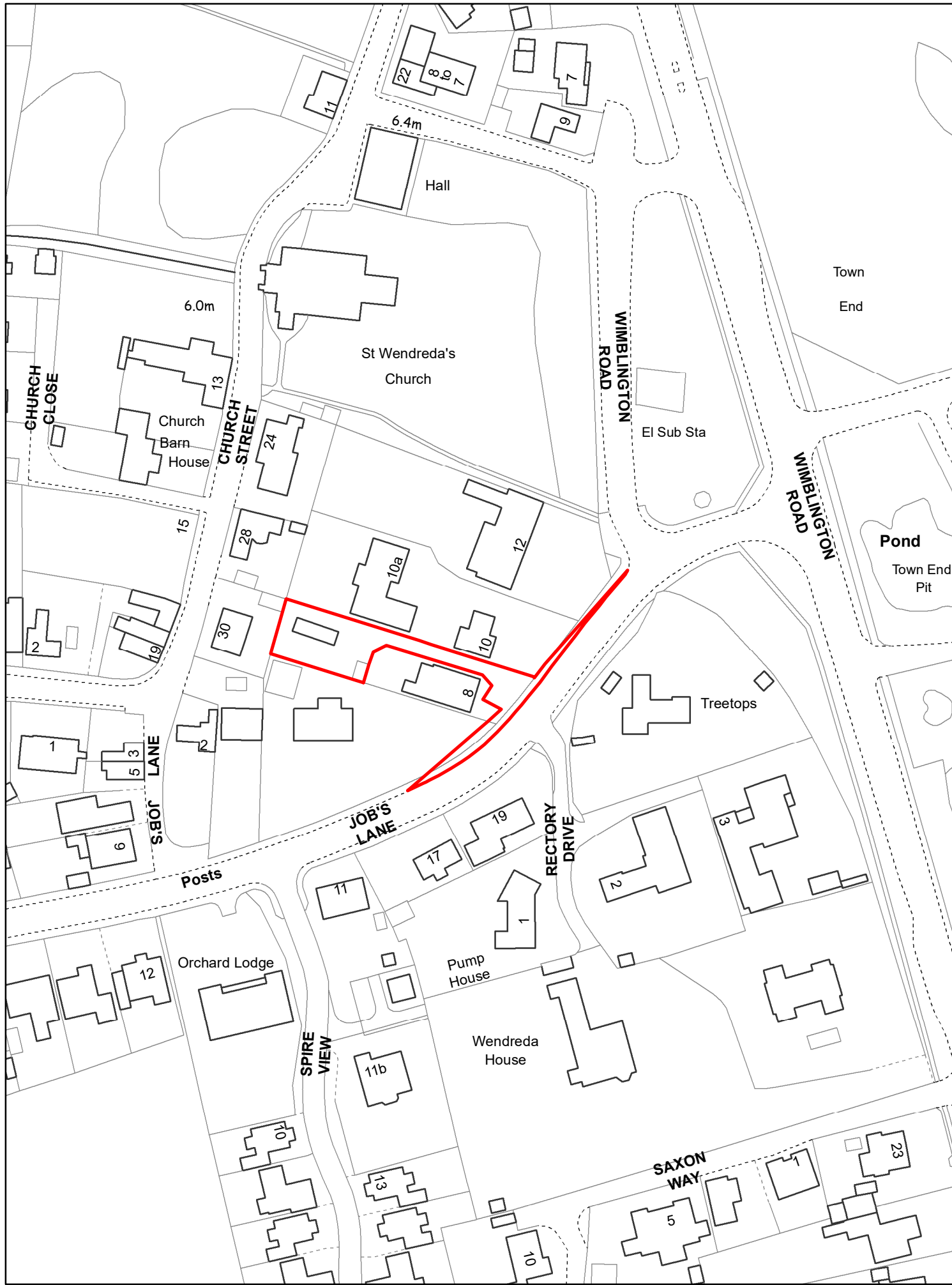
11 RECOMMENDATION

Grant subject to the following conditions;

Conditions

1	<p>Approval of the details of:</p> <ul style="list-style-type: none"> i. the layout of the site ii. the scale of the building(s); iii. the external appearance of the building(s); iv. the landscaping <p>(hereinafter called "the Reserved Matters") shall be obtained from the Local Planning Authority prior to the commencement of development.</p> <p>Reason - To enable the Local Planning Authority to control the details of the development hereby permitted.</p>
2	<p>Application for approval of the Reserved Matters shall be made to the Local Planning Authority before the expiration of 3 years from the date of this permission.</p> <p>Reason - To ensure compliance with Section 92 of the Town and Country Planning Act 1990.</p>
3	<p>The development hereby permitted shall begin before the expiration of 2 years from the date of approval of the last of the Reserved Matters to be approved.</p> <p>Reason - To ensure compliance with Section 51 of the Planning and Compulsory Purchase Act 2004.</p>
4	<p>The residential elements of the development shall not exceed 1 dwelling (Use Class C3).</p> <p>Reason - For the avoidance of doubt and to ensure a satisfactory standard of development.</p>
5	<p>The landscaping details to be submitted in accordance with Condition 01 of this permission shall include:</p> <ul style="list-style-type: none"> a) existing and proposed site levels including those on adjacent land. b) means of enclosure ensuring that adequate gaps are provided under any new fencing to allow for the passage of hedgehogs. c) car parking, vehicle and pedestrian access and circulation areas (which shall be of a bound material) d) hard surfacing, other hard landscape features and materials e) planting plans, including specifications of species, sizes, planting centres number and percentage mix, a range of native tree and shrub species should be included. f) details of planting or features to be provided to enhance the value of the development for biodiversity and wildlife g) existing trees, hedges or other soft features to be retained

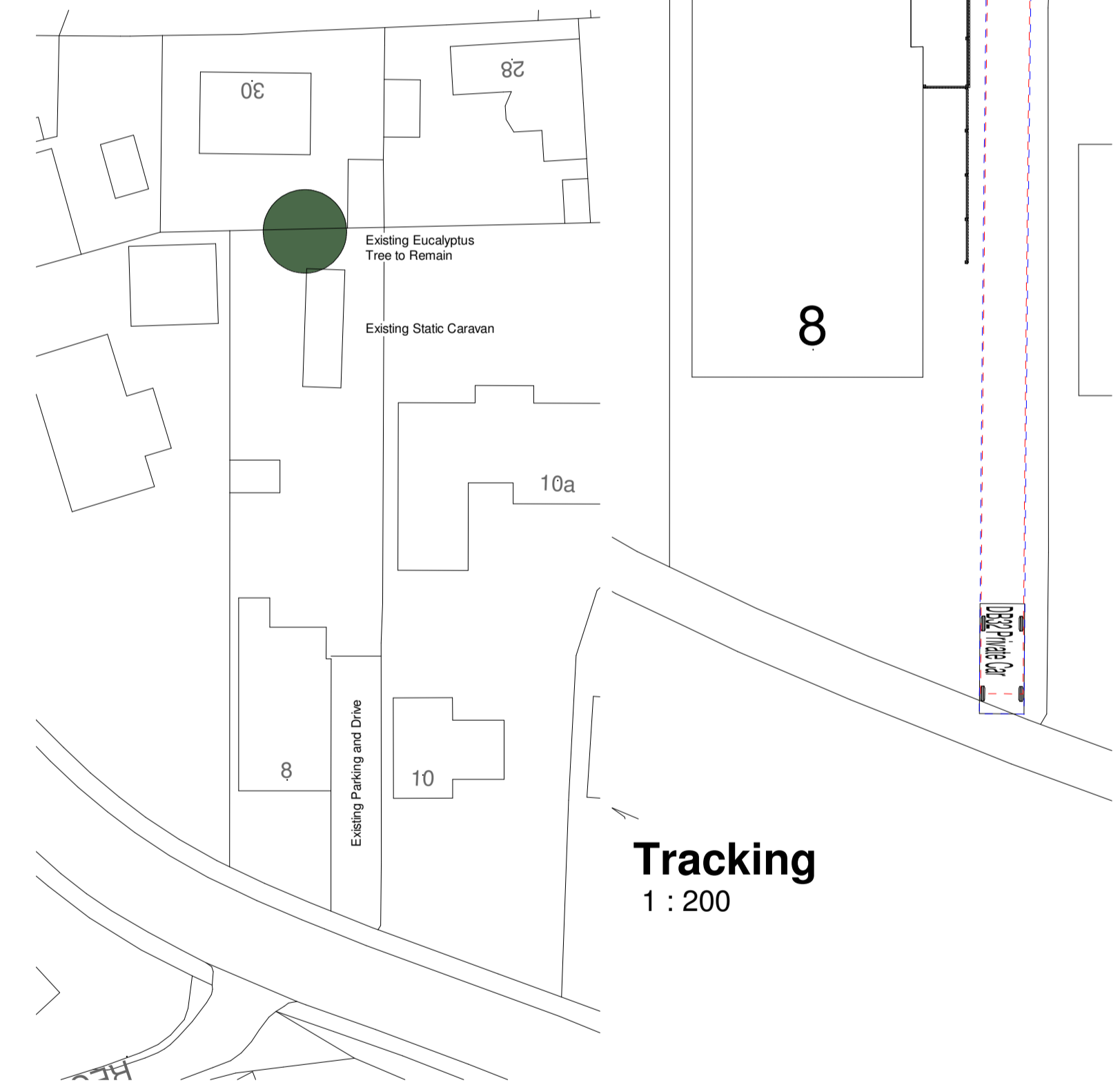
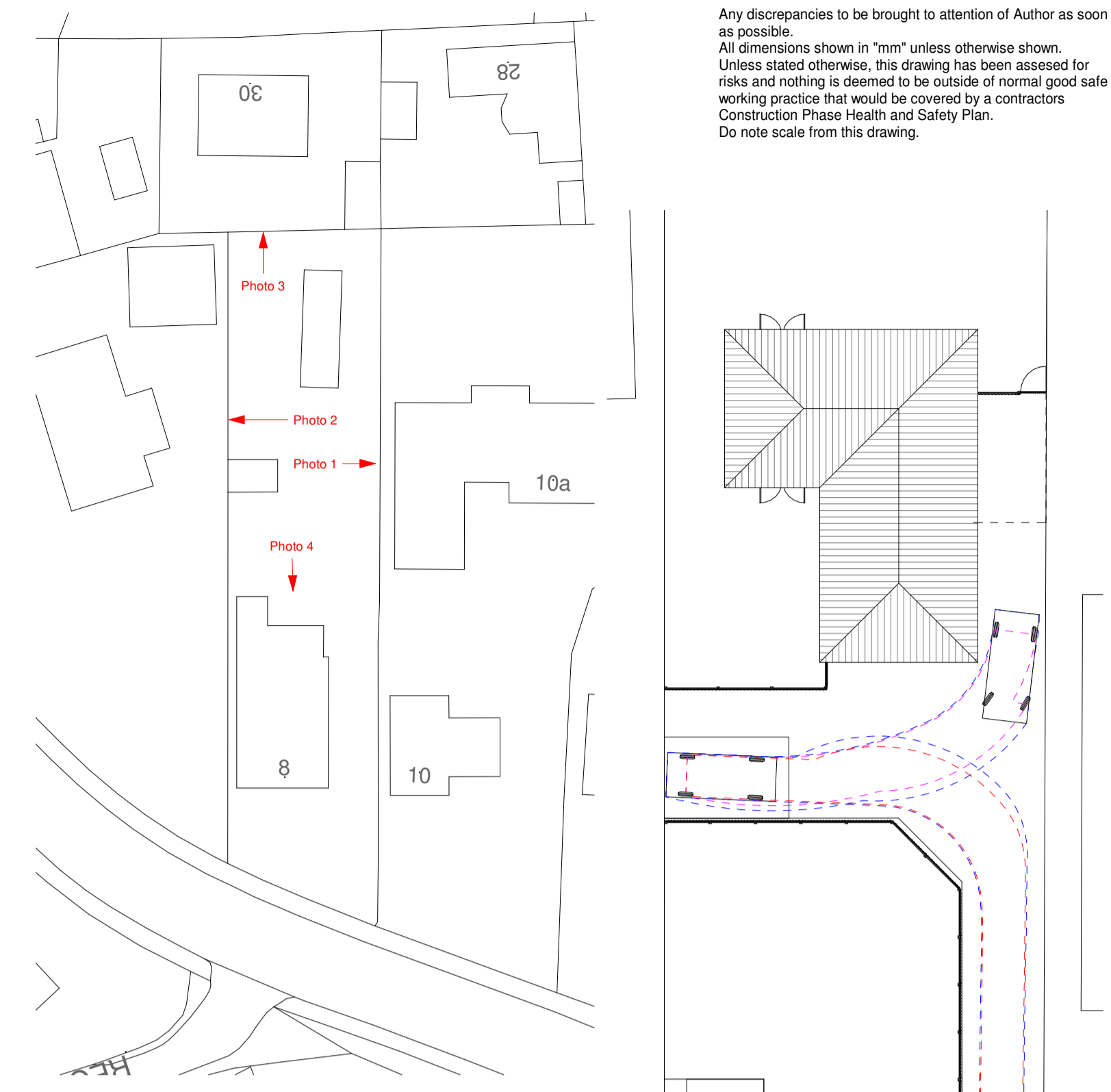
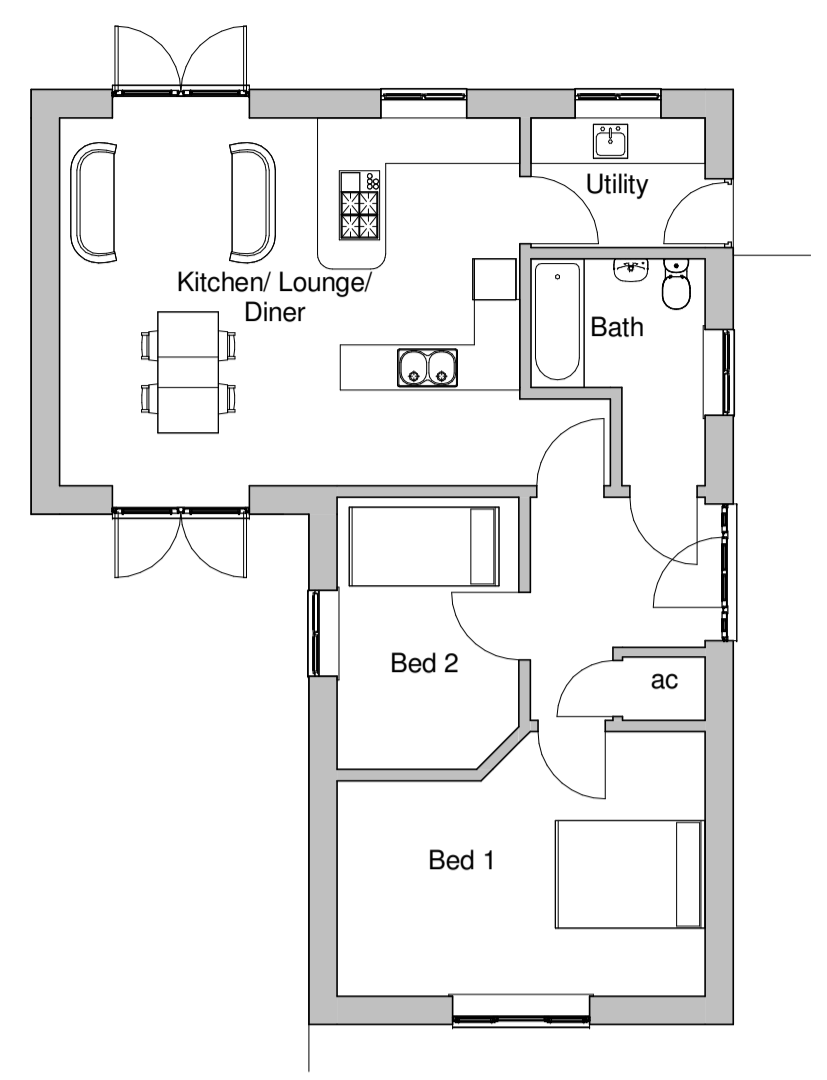
	<p>h) timing of landscaping works</p> <p>All works shall then be carried out in accordance with the approved details.</p> <p>Reason - To ensure the appearance of the development is satisfactory and contributes to the visual character and amenity of the area and to protect the character of the site and enhance biodiversity in accordance with Policy LP16 of the Fenland Local Plan, 2014.</p>
6	<p>If, during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the Local Planning Authority) shall be carried out until the developer has submitted, and obtained written approval from the Local Planning Authority detailing how this unsuspected contamination shall be dealt with. The development shall then be carried out in full accordance with the approved remediation strategy.</p> <p>Reason: To control pollution of land and controlled waters in the interests of the environment and public safety in accordance with the National Planning Policy Framework, in particular paragraphs 178 and 179, and Policy LP16 of the Fenland Local Plan 2014.</p>
7	<p>Prior to the first occupation of the development the proposed access from Jobs Lane and on-site parking/turning area (for both the proposed and host dwellings) shall be laid out in accordance with the approved plans, surfaced in a bound/sealed material and drained within the site. The access, parking/turning area, surfacing and drainage shall thereafter be retained as such in perpetuity (notwithstanding the provisions of Schedule 2, Part A, Class F of The Town and Country Planning (General Permitted Development) (England) Order 2015, or any instrument revoking or re-enacting that Order).</p> <p>Reason - In the interests of highway safety in accordance with Policy LP15 of the Fenland Local Plan 2014.</p>
8	<p>Prior to the first occupation of the development hereby permitted, the visibility splays, as detailed on Drawing 382-P01 E, shall be provided and maintained free from any obstruction over a height of 600 mm above the level of the footway.</p> <p>Reason - In the interests of highway safety in accordance with Policy LP15 of the Fenland Local Plan 2014.</p>
9	<p>The development hereby permitted shall be single-storey only and there shall be no accommodation in the roof.</p> <p>Reason - To safeguard the amenities currently enjoyed by the occupants of adjoining dwellings in accordance with Policies LP2 and LP16 of the Fenland Local Plan 2014.</p>
10	Approved Plans



Created on: 03/09/2020
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F/YR20/0576/O
 Scale = 1:1,250





Tracking
1 : 200

Location Plan
1 : 2500

North

E	Pedestrian Visibility Splay Added	21-08-20
D	Block plan amended	14-08-20
C	Visibility splay amended	12-08-20
B	Parking amended	17-07-20
A	Indicative added to site plan	06-07-20

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Proposed Outline Application for a new Single Storey Dwelling at Land Rear, 8 Jobs Lane, March for Lisa Shepherd

Planning Drawing

date created	scale	drawing no.	rev.
11/06/20	As indicated @ A1	382 - P01	E

Notes
Any discrepancies to be brought to attention of Author as soon as possible.
All dimensions shown in "mm" unless otherwise shown.
Unless stated otherwise, this drawing has been assessed for risks and nothing is deemed to be outside of normal good safe working practice that would be covered by a contractors Construction Phase Health and Safety Plan.
Do not scale from this drawing.

PLANNING APPEAL DECISIONS

The Council has received the following appeal decisions in the last months. All decisions can be viewed in full at <https://www.fenland.gov.uk/publicaccess/> using the relevant reference number quoted.

Planning Application Reference: (Appeal reference) ENF/012/17/UCU (Appeal references: Appeal A: APP/D0515/C/19/3224948 & Appeal B: APP/D0515/C/19/3224949					
Site/Proposal: Breach of planning control as alleged in the notice is without the benefit of planning permission, the material change of use to mixed use as restaurant/bar/nightclub and snooker hall and the erection of 2.47 metre high fence to rear. 2 Chapel Road, Wisbech, Cambridgeshire PE13 1RG					
Officer Recommendation:	N/a	Decision Level:	N/a	Appeal Decision:	Dismissed
Main Issues:					
<p>An enforcement notice was issued on 21st February 2019 alleging a breach of planning control, as indicated in the above description. The requirement of the notice was to cease the unauthorised use of the premises with a 3-month compliance period. The grounds of appeal were as follows:</p> <ul style="list-style-type: none"> • The appellants argued that the matters alleged in the notice had not occurred • Furthermore the appellants considered that a breach of planning control had not occurred • In addition they alleged that the Enforcement Notice was outside the prescribed timeframes for enforcement action, and • Considered that deemed planning permission should be granted • Finally the appellants considered that the steps required by the notice were excessive 					
Summary of Decision:					
<ul style="list-style-type: none"> • The Inspector found that the the matters alleged in the notice had occurred noting that: <ul style="list-style-type: none"> - The premises were in 'mixed use' as opposed to operating as two separate planning units, and that this 'mixed use' should be considered 'sui generis' as it comprised a number of <i>'disparate activities'</i>. - Based on his site inspection and other available evidence the Inspector noted that a nightclub was operating at the ground floor of the premises, along with a restaurant, bar and club • The Inspector noted that the Use Classes Order explicitly excluded use as a nightclub from any of the classes specified and that the use operating was a nightclub, as opposed to a Class D2 - dance hall; however as indicated above he reiterated that a mixed use is classified as sui generis even if individual components fall within a described use class. Furthermore the Inspector highlighted that each time a new primary use was introduced a material change of use would have occurred. As such there was a breach of planning control. 					

- With regard to the **legitimacy of the service of the enforcement notice** the Inspector identified that to succeed on this ground the appellant would have to demonstrate that the change of use to a 'mixed use' had occurred at least 10 years before the issue of the enforcement notice (21st February 2019) and had continued without material interruption throughout that period. It was highlighted in the Appeal decision that the appellant had provided no substantive evidence in this regard and that based on information provided on the earlier planning applications and available social media information it was clear that the premises opened as a nightclub on 31st December 2016. As such the appellants had not demonstrated that the change of use had occurred at least 10 years prior to the the issue of the Enforcement Notice.

- In considering **whether deemed planning permission should be granted** the Planning Inspector considered that the main issue was the effect of the change of use on the living conditions of residential occupiers within the vicinity of the appeal site; with particular reference to noise and disturbance. In summary the Inspector noted that:

- Although a premises licence had been issued these were different regimes and that the licence had been issued prior to the opening of the venue at ground floor
- The appellants had not appealed the earlier refusal of planning permission
- Noise levels had previously been monitored from inside a neighbouring property and the recording taken demonstrated noise levels in the early hours which the FDC Environmental Protection team (EP) concluded unreasonable. The Inspector noted in this regard that there appeared to be a direct conflict in professional views between the noise consultant engaged by the appellant and EP at the time of the planning application.
- In the absence of any updated data or noise report since the planning application was dismissed the Inspector highlighted that *'the technical evidence in support of the use [was] simply too vague for reliance to be placed on it'* and that in his mind *'it is too late for a noise impact assessment to be conducted once permission has already been granted with the risk that suitable mitigation measures cannot be achieved'*.

Based on the above the Inspector found the use to be contrary to Policies LP2 and LP16 and Para 127(f) of the NPPF as he was not satisfied that noise and disturbance caused by the mixed use could be controlled to acceptable levels, including through the use of conditions; accordingly the application for deemed planning permission failed.

- In considering **whether the steps required in the notice were excessive** the Inspector found that:

- If he were to vary the notice to require the nightclub use to cease at ground floor only this would result in unconditional planning permission being granted once the remaining requirements were complied with which would render the nightclub use free to operate on the first floor free from any planning restrictions including opening times.
- Such an approach would also not be consistent with the development enforced against as the nightclub use was not in operation at first floor at the time that the enforcement notice was served; furthermore there was no technical evidence to demonstrate whether noise attenuation measures could be resolved
- although the appellants submit that noise mitigation could be achieved at ground floor and that this could be achieved through S106 or Unilateral Undertaking; however as no such deed accompanied the appeal documentation this could not be taken forward. As indicated elsewhere in the decision the Inspector was not satisfied that noise mitigation would be effective in protecting against noise and disturbance.

- Although the appellants had suggested a number of alternative approaches to how the uses were delivered across the two floors these were outside the scope of what could be considered as part of the Appeal
- Based on the above the Inspector did not feel that the requirements of the notice to remedy the breach were not excessive.
- The Inspector did however extend the period of compliance to allow time for the Appellant to explore alternative options to be explored with the council through the submission of a planning application; although noting that this did not indicate that any of the options mooted were acceptable as this remained a matter for determination by the LPA.

Planning Application Reference: F/YR19/0562/F					
(Appeal reference: APP/D0515/W/20/3247455)					
Site/Proposal: Erect 1 dwelling (3-bed, 2-storey) at Land South West of 38 Burnsfield Estate accessed from Treeway, Chatteris					
Officer Recommendation:	Refuse	Decision Level:	Delegated	Appeal Decision:	Dismissed
Main Issues:					
<ul style="list-style-type: none"> • Character and Appearance • Highway safety 					
Summary of Decision:					
<p>Character and appearance</p> <p>The Inspector concluded that that the proposed house would be an incongruous feature, out of place in the street scene. It would be set back significantly from Treeway and would not accord with the character of the nearest terraced dwellings to which the development site most closely relates.</p> <p>Also found that the proposed house would present a blank side elevation to the approach along Treeway from Station Road and considered that this would result in harm to the street scene due to the lack of visual interest. While acknowledging examples of similar elevations do exist elsewhere in Treeway, concluded that the presence of such does not justify a development that would cause harm to the character of the area.</p> <p>Found the development would conflict with Policy LP16 of the Fenland Local Plan May 2014 acknowledging that this policy requires, amongst other things, that new development make a positive contribution to the local distinctiveness and character of the area.</p> <p>Highway safety</p> <p>Considered that vehicle access to the site would be constrained by the narrow width of the entrance, and the presence of a speed calming feature opposite that entrance. This would be likely to result in cars having difficulty manoeuvring onto and off the site, obstructing the passage of other road users and pedestrians.</p>					

Noted that the site has high fences up to the pavement edge which would limit visibility for vehicles leaving the site and while these could be reduced on the appeal site, the high fence to No 12 Treeway (adjacent) would similarly restrict visibility, and no evidence to indicate that reduction of this fence falls within the appellants' control.

Concluded that the development conflicts with Policy LP15 which requires, amongst other things, that development provides well designed, safe and convenient access for all.

Other considerations

The inspector considered the benefits of the development; it would contribute to the local housing supply, and there would be social benefits from its occupation and financial benefits from its construction and occupation. The appeal site is in a sustainable location within an existing settlement, with access to services and facilities. However, considered that these benefits would be limited as the proposal is for a single dwelling, and did not consider that they would outweigh the identified harm.